

VILLAGE OF IRVINGTON
NEW YORK



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MARIANNE STECICH
VILLAGE ATTORNEY

January 23, 2009

Dear Constituents:

In 2003, the Irvington Comprehensive Plan called for the re-zoning of the Industrial District, which encompassed the area of the Village at the confluence of Main Street and the Hudson River. Specifically, the Comprehensive Plan recommended a mix of residential and business uses to replace the outdated and largely undesirable uses allowed by the Industrial District zoning. Subsequent to the completion of the Comprehensive Plan, a portion of the Industrial District was, in fact, re-zoned to be part of the Business District, while the portion to the west of the Hudson Line railroad remained unchanged. In the years following the release of the Comprehensive Plan, the Irvington Board of Trustees undertook the study of various ways to re-zone the remaining Industrial District.

During that process, in February 2007, Bridge Street Properties, LLC, the only private-property owner of the land west of the railroad, presented the Board of Trustees with a petition to change the zoning for one of their waterfront parcels to facilitate the development of 19 town homes along with retail space along West Main Street. The parcel in question is located between West Main Street and the Irvington Boat Club and currently contains a large parking lot along with some smaller buildings.

The Draft Irvington Waterfront Study being released today was prepared by BFJ Planning at the request of the Board of Trustees in response to the zoning petition and development proposal submitted by Bridge Street Properties, LLC. The study also provides zoning recommendations applying to the larger waterfront area including the remaining waterfront areas currently zoned Industrial. This study was prepared to help the Board of Trustees evaluate the Bridge Street Properties zoning petition and development proposal.

This draft study contains important information that will be useful for future re-zoning efforts. **However, as of its publication today, this study does not represent the position of, and has not been endorsed by, the Board of Trustees.**

In order to satisfy the statutory requirements of Section 224-99 of the Irvington Zoning Code, **the Board of Trustees will hold a public hearing on February 23, 2009 at 8 p.m. in Village Hall** for the sole purpose of considering the Bridge Street Properties zoning petition.

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January 23, 2009

Should the Board of Trustees not accept the Bridge Street Properties zoning petition, it will still be necessary to develop appropriate legislation to re-zone the waterfront Industrial property. The Board of Trustees is committed to undertaking a process for re-zoning that includes significant public involvement along with full consideration of state-of-the-art waterfront zoning techniques and principles. A series of public meetings will be held this spring to solicit public input for the parcels in question.

The Board of Trustees invites you to review this study and attend the February 23rd public hearing and/or submit comments. Please forward your comments to Village Administrator Lawrence Schopfer at Village Hall, 85 Main Street, Irvington, NY or by email at LSCHOPFER@IRVINGTONNY.GOV.

Sincerely,
Nicola Coddington, Mayor
John Malone, Trustee
Terence Masterson, Trustee
Walter Montgomery, Trustee
Jonathan Siegel, Trustee



DRAFT

IRVINGTON WATERFRONT STUDY
Village of Irvington, NY

Prepared for:
Irvington Board of Trustees

Prepared by:
BFJ Planning
115 Fifth Avenue
New York, NY 10003
In Association with:
Gregg C. DeAngelis Architects

January 2009

This draft study was prepared by BFJ Planning at the request of the Village Board of Trustees to help them evaluate a zoning amendment petition and development proposal submitted by Bridge Street Properties, LLC on February 13, 2007 for a 3- acre waterfront parcel located in the Village of Irvington, New York. As of the date of its publication by the Board of Trustees (January 23, 2009), this study does not represent the position of, and has not been endorsed by, the Board of Trustees.

DRAFT Irvington Waterfront Study
Village of Irvington, NY

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1.0 Introduction and Existing Conditions

This report was prepared in response to a zoning petition and development proposal for a 3-acre waterfront parcel (the "North Parcel") located in the Village of Irvington, New York. It also provides zoning recommendations applying to the larger waterfront area including the South Parcel which is currently mapped Industrial District (I). The report was prepared with the guidance of the Village of Irvington. Chapter 1.0 reviews existing conditions and summarizes the concept plan proposed by Bridge Street Properties for the North Parcel, Chapter 2.0 reviews regulatory controls including the comprehensive plan, zoning, and floodplain, Chapter 3.0 provides a visual analysis of the North Parcel concept plan, Chapter 4.0 reviews downtown and waterfront developments in comparable Westchester communities to put the scale of the North concept plan in context, Chapter 5.0 provides recommendations relating to the North Parcel concept plan and Chapter 6.0 recommends a new Waterfront Mixed Use (W) to replace the Industrial (I) zone on both North and South Parcels.

Background

In 2003 the Village prepared a new comprehensive plan that recommended the elimination of the industrial zoning mapped on the waterfront including both North and South Parcels. In the same year, Bridge Street Properties, the owners of both North and South parcels, submitted a request for a zone change including a concept plan for a new mixed use development to replace the existing surface parking lot at the North Parcel. In the summer of 2006, a special meeting of the Village Board was held at the public library to receive public input on the redevelopment of the project site. The results of the special meeting are summarized in the appendix to this report.

Public comments at the special meeting focused on potential impacts to the viewsheds from Main Street and Matthiessen Park to the Hudson River, restricting building heights, and maintaining and improving public access. Comments were also made that new development should integrate smoothly with the Village, water and park and should be consistent with Main Street's character and style. Priorities also focused on providing sufficient parking and improving the current appearance with sidewalks, landscaping, benches, lighting and pedestrian facilities.

1.1 Site Description

The North Parcel lies at the foot of Main Street on the west side of Metro North railroad and immediately on the waterfront. It includes approximately 2.4 acres of land with an additional 0.6 acres under water. The site includes two smaller structures including a maintenance building and the Pateman office building. The majority of the site is used as an approximately 242-space surface parking lot. To the immediate north of the site is the Boat Club which is a separate parcel and also owned by Bridge Street Properties. The Village-owned Matthiessen Park extends to the north of the Boat Club.

To the south is West Main Street, a Village-owned street and south of that are four commercial buildings, three of which were formerly part of the Lord and Burnham manufacturing complex and today are owned by Bridge Street Properties. The buildings have been adaptively reused, and today they each contain a total of 175,000 square feet occupied by commercial and office uses. In 2002, an additional, 3 story, 25,000 square foot commercial building was constructed bringing the total up to 200,000 square feet. These buildings are currently served by surface parking including spaces on the North Parcel (see Section 3.2 of this report which addresses existing parking). These existing commercial uses are part of the South Parcel and cover a total of approximately 5.5 acres. To the south of the Bridge Street properties is Scenic Hudson Park which includes ballfields and public open space.

1.2 North Parcel Concept Plan and Zoning Petition

The concept plan for the North Parcel calls for 19 residential units, including 13 townhouses and 6 duplexes, above approximately 4,500 square feet of ground floor commercial space fronting West Main Street¹. An approximately 226-space, four-level parking garage is proposed with access via ramps from Bridge Street. The proposal is described fully in Chapter 3.0 of this report along with a site plan (see Figure 9).

Zoning and other regulatory controls are discussed in the next section of this report. Chapter 6.0 of this report proposes a new Waterfront Mixed Use (W) zone that would be mapped on the North and South Parcels. This responds to Bridge Street Properties' proposed zoning and incorporates recommendations from the Village of Irvington who guided the preparation of this report.

A site location map, aerial and site photographs follow.

¹ Petition for the Creation of a Mixed-Use District and Zoning Map Amendment, Shamberg Marwell Davis & Hollis, P.C. on behalf of Bridge Street Properties LLC, February 13, 2007.



FIGURE 1: PROJECT LOCATION

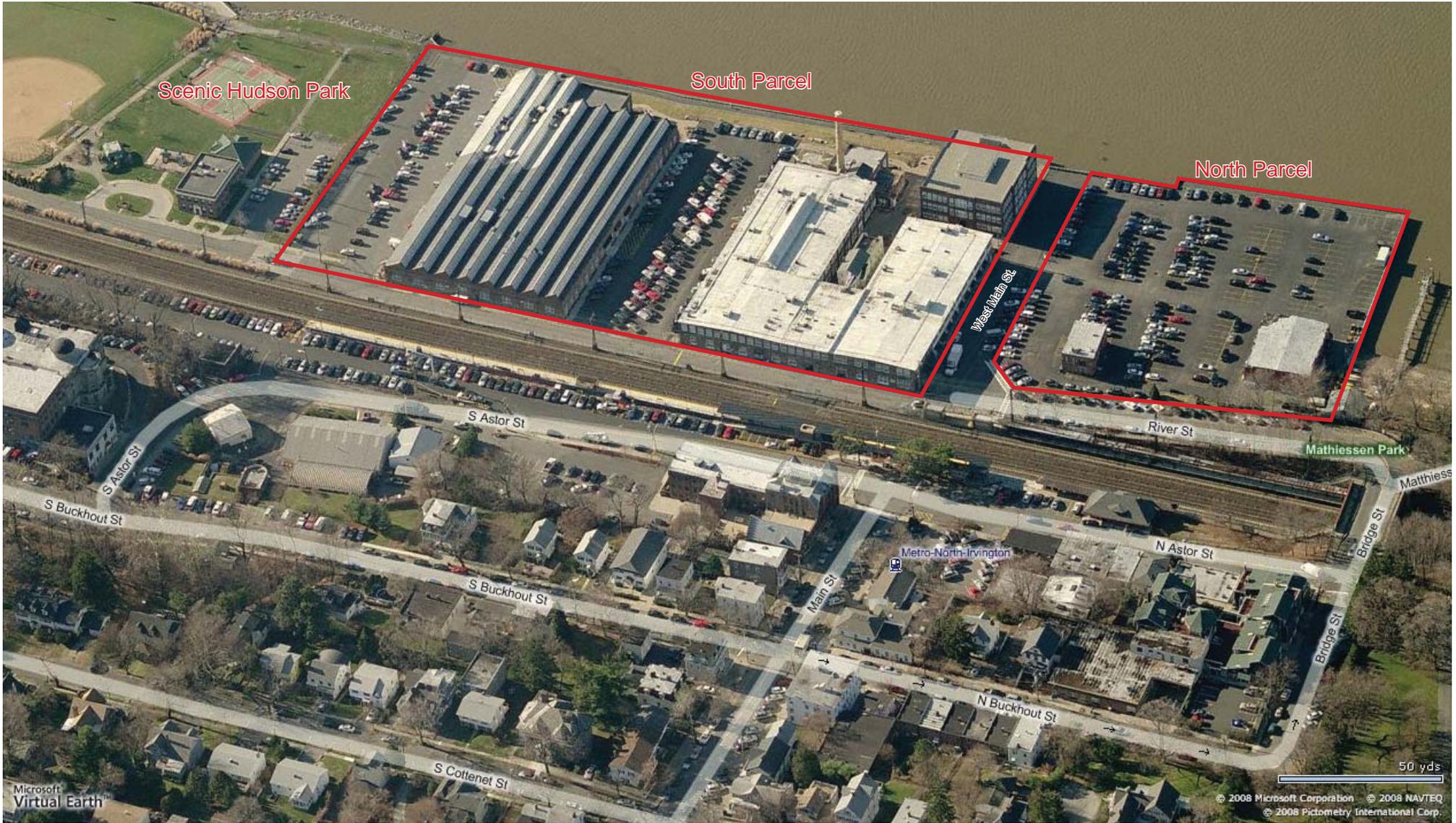


FIGURE 2: AERIAL MAP



FIGURE 3: SITE PHOTOS



FIGURE 4: MAIN STREET PHOTOS



FIGURE 5: MATTHIESSEN PARK PHOTOS

2.0 Zoning and Other Controls

2.1 Village Comprehensive Plan

The Village's 2003 Comprehensive Plan (the "Plan") contains a number of recommendations relating to the project site. It recognizes that the area along the waterfront is primarily used as office and restaurant space and recommends eliminating the Industrial zoning to accommodate future growth and improvements that are compatible with the Village's goals. Additional priorities of the Plan are to improve public access to the waterfront and continue the adaptive reuse of the existing Lord and Burnham warehouses and manufacturing buildings that are an essential component of the area's character. The Plan also recognizes the need to improve pedestrian access to Scenic Hudson Park and notes the negative impacts of the surface parking at the North Parcel.

Chapter 6.0 of the Comprehensive Plan specifically addresses the Main Street area and waterfront, and recommends preserving views of the Hudson River, improving pedestrian and vehicular access to the waterfront, designating a pedestrian pathway to Scenic Hudson Park, improving the streetscape along West Main Street extending to the waterfront and the elimination of the Industrial zoning.

2.2 Zoning

Figure 6 shows the existing zoning map: an I* at the waterfront shows the extent of the Industrial District Zoning. The asterisk signifies the Village's View Preservation Overlay District described in Article XXIV of the Village Code. This overlay provides that the Planning Board shall review site plan applications in the VP district "as to the best siting, dimensions and configuration of principal and accessory structures so as to cause the least possible obstruction of the view of the Hudson River for neighboring properties and adjacent public property and rights of way." Table 7 summarizes the proposed MU zone requirements as described in the zoning petition as compared to the existing controls within the neighboring Business District (B) and Industrial District (I). We note that residential uses are permitted within the Business District at a density of 1 unit per 2,500 square feet of lot area and that the zoning petition proposes half this density at 1 unit per 5,000 square feet of lot area. FAR is proposed at 1.4, the same as is currently permitted within the Industrial District.

Existing Built Floor Area

A rough estimate is that One Bridge Street and 2 West Main Street (the new three story building) are currently built out at 1.4 FAR. This assumes a 2.02 acre parcel². 2 Bridge Street to the south on a roughly 3.5 acre parcel is built at about 0.35 FAR, leaving 1.05 FAR of potential development. Both sites are constrained by parking requirements however, which is described in the next section.

² Parcel sizes are based on Village tax maps which are included in the appendix to this report.

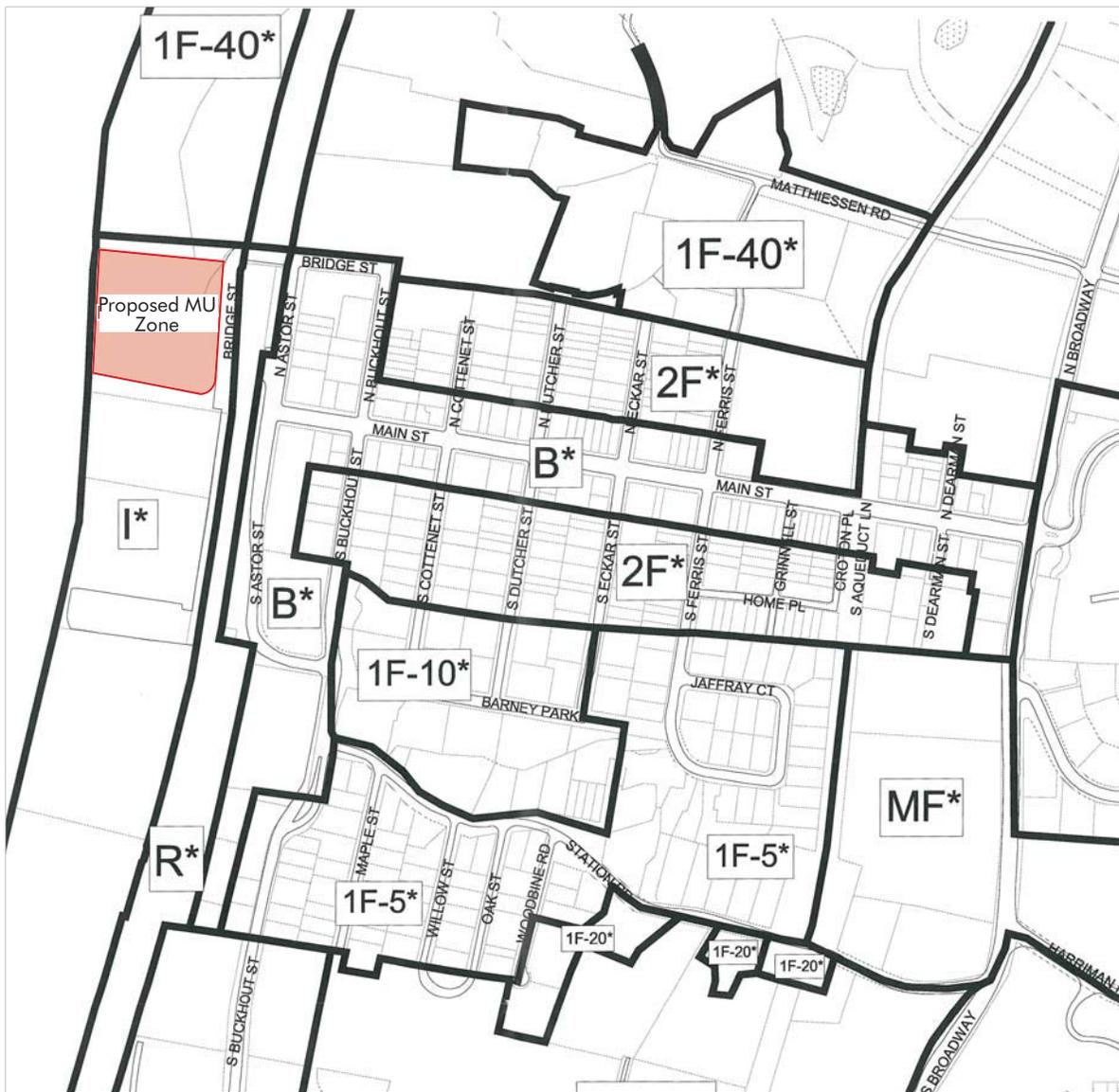
2.2 Parking

The North Parcel currently includes approximately 242 parking spaces. This includes 17 spaces required for the Pateman office building, plus 186 spaces for the South Parcel including the new 2 West Main Street Building. Based on an on-site survey on a weekday during spring 2008, we noted that 25 percent of the spaces were not used. As part of our work for this report, the Village of Irvington requested that we review the feasibility for a new parking deck that would be located immediately south of One Bridge Street as an alternative to the concept proposed by Bridge Street Properties. A memo addressing this is included in the appendix. We conclude that such a scheme is inefficient, would block light and air to adjacent buildings and is not cost effective.



<u>Bldgs. South of Main St.</u>	<u>Required Parking</u>	<u>Parking Provided On-Site</u>
One Bridge St.	236	106 space surface lot
Two Bridge St.	91	<u>105</u> space surface lot
Boiler Room	10	
West Main St.	<u>60</u>	
Total:	397	211

<u>North Parcel</u>	<u>Required Parking</u>
Pateman Bldg.	7
Maintenance Bldg.	10
Spaces for South Parcel	186
Additional Parking	<u>39</u>
Total:	242



LEGEND

-  Zones
-  Parcels
-  Roads
-  Lakes/Ponds

ZONE DESCRIPTION

- 1F-40** One Family Residence District
- 1F-5** One Family Residence District
- 2F** Two Family Residence District
- MF** Multi-Family Residence District
- B** Business District
- I** Industrial District
- R** Railroad District

* All Parcels West of Broadway are Designated as Part of the View Preservation Overlay District (VP)

FIGURE 6: ZONING MAP

Figure 7: Summary of Existing Business and Industrial District Zoning Controls

Zoning Controls	Business District (B) Article IX	Industrial District (I) Article X	Mixed Use (MU) as proposed by BSP*
Permitted Uses	Commercial, office, retail and restaurant uses Residential is permitted at 1 unit/2,500 sf of lot area, must be located 2 nd floor or higher	Industrial uses Residential by special permit	Retail stores, banks not more than 5% of all bldgs. on lot, business, offices Residential by BOT special permit. 1/5,000 s.f. of dry land area of lot.
Floor Area Ratio	n/a (only applies to residential districts)	1.4 (excludes storage in basements, cellars)	1.4, based on dry land area of lot 0.6 FAR residential 1.0 parking structure
Height	35 ft or three stories No higher than neighboring bldgs. on Main Street	35 ft or three stories	35 feet or three stories 28 feet for parking structure
Coverage	Principal and accessory bldgs. 70% Incl. parking and driveways shall not exceed 80%	Principal and accessory bldgs. 70% Residential bldgs. 40%.	50% principal and accessory bldgs, 80% for bldgs. plus parking and driveways
Yard requirements	At least 4 ft wide, if req'd by Planning Board At least 10 ft wide abutting res. district.	None required, but, if provided any yard shall be at least 4 ft wide At least 10 ft wide abutting res. district.	Green zone: 60 feet and 24 feet deep on Hudson River, average of not less than 43 feet. Private terraces may encroach. Min. 10 foot on Bridge Street Min. 10 feet on West Main Street Parking garage: min. 15 feet from West Main, 10 feet from Bridge
Parking	1 space/1 bdrm, 1.5 spaces/2 bdrm., 2 spaces/3+bdrm. 15% credit w/in 600' of RR station tunnel 1 space/150 sq. ft. retail 1 space/200 sq. ft. office space Credit for 24' of permitted curb parking	1 space/DU 1 space/500 sq. ft. on first floor 1 space/375 sq. ft. on second and higher	2 spaces/DU 1 space/300 sq. ft. of retail/service 1 space/200 sq. ft. of medical, dental

*Notes: Mixed Use zone based on 2/13/07 zoning petition from Bridge Street Properties (BSP)

Overlay Zone

Article XXIV View Preservation Overlay	Denoted by * on zoning map. Planning Board reviews siting, dimensions, configuration of site plans so as to cause least possible obstruction of views to Hudson River. Application may be referred to Board of Architectural Review.
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Section 224-163 Greenway Compact Plan	To extent that Village amends or enacts new land use laws, such laws, where appropriate would be consistent with Compact Plan
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2.3 Floodplain and Climate

A concern raised by the Village of Irvington focused on climate change and potential sea level rise and how this might impact the Village's waterfront and redevelopment of the North Parcel. This is an important issue for many coastal area communities. In our knowledge there is no local zoning authority granted to municipalities in New York State to regulate waterfront development because of potential sea level rise. Regulation instead focuses on storm events and ensuring minimum damage as a result of flooding during major storm events (such as a 100 year flood). Revised Federal Emergency Management Agency (FEMA) flood maps and an amended flood control law were adopted by the Village last year in September 2007. Below are the objectives of the Village's flood control laws as described in Section 124-3:

- A. Protect human life and health.
- B. Minimize expenditure of public money for costly flood control projects.
- C. Minimize the need for rescue and relief efforts associated with flooding and generally undertaken at the expense of the general public.
- D. Minimize prolonged business interruptions.
- E. Minimize damage to public facilities and utilities such as water and gas mains, electric, telephone and sewer lines, streets and bridges located in areas of special flood hazard.
- F. Help maintain a stable tax base by providing for the sound use and development of areas of special flood hazard so as to minimize future flood blight areas.
- G. Provide that developers are notified that property is in an area of special flood hazard.
- H. Ensure that those who occupy the areas of special flood hazard assume responsibility for their actions.

Any redevelopment of the parcels within the flood zones as described on the updated flood maps will have to demonstrate compliance with the provisions of this section. As Figure 8 indicates, a large portion of the north parcel is located within the 100 year flood plain and within flood zone AE at elevation 7 feet. Under the latest New York State DEC guidelines and local laws adopted by the Village, habitable space must be located 2 feet above base flood elevations mapped for the 100 year flood.

In addition to the Village's existing flood laws, we recommend adding new zoning language to clarify for developers, property owners and business owners that development within the Village's Waterfront Mixed Use district (W) must comply with the provisions of Chapter 124 Flood Damage Prevention of the Village code.

Other local measures at tackling climate change focus on energy conservation, promoting green buildings and lobbying. For example the U.S Conference of Mayors Climate Protection Agreement calls for cities to strive to meet the Kyoto protocol including a 7% reduction from 1990 levels by the year 2012. This agreement has already been signed by over 500 communities nationwide, including White Plains, New Rochelle, Yonkers, Tarrytown and NYC. If the village joins ICLEI-US and makes the commitment to participate in the Cities for Climate Protection Campaign, ICLEI then provides experienced

staff, software tools, and a wide variety of programs and technical assistance to help reduce greenhouse gas emissions³. Westchester County also has a Global Warming Taskforce made up of representatives from government, business, schools and colleges and the environmental community so that each sector can address both short-term and long-term actions specific to their area. It is chaired by North Castle Supervisor Reese Berman and co-chaired by Robert Funicello, environmental project director of Westchester County⁴.

³ <http://www.iclei.org/index.php?id=1120#milestones>

⁴ http://www.westchestergov.com/environment_TaskForce.htm

LEGEND

SPECIAL FLOOD HAZARD AREAS SUBJECT TO INUNDATION BY THE 1% ANNUAL CHANCE FLOOD

The 1% annual flood (100-year flood), also known as the base flood, is the flood that has a 1% chance of being equaled or exceeded in any given year. The Special Flood Hazard Area is the area subject to flooding by the 1% annual chance flood. Areas of Special Flood Hazard include Zones A, AE, AH, AO, AR, A99, V, and VE. The Base Flood Elevation is the water-surface elevation of the 1% annual chance flood.

- ZONE A** No Base Flood Elevations determined.
- ZONE AE** Base Flood Elevations determined.
- ZONE AH** Flood depths of 1 to 3 feet (usually areas of ponding); Base Flood Elevations determined.
- ZONE AO** Flood depths of 1 to 3 feet (usually sheet flow on sloping terrain); average depths determined. For areas of alluvial fan flooding, velocities also determined.
- ZONE AR** Special Flood Hazard Area formerly protected from the 1% annual chance flood by a flood control system that was subsequently decertified. Zone AR indicates that the former flood control system is being restored to provide protection from the 1% annual chance or greater flood.
- ZONE A99** Area to be protected from 1% annual chance flood by a Federal flood protection system under construction; no Base Flood Elevations determined.
- ZONE V** Coastal flood zone with velocity hazard (wave action); no Base Flood Elevations determined.
- ZONE VE** Coastal flood zone with velocity hazard (wave action); Base Flood Elevations determined.

FLOODWAY AREAS IN ZONE AE

The floodway is the channel of a stream plus any adjacent floodplain areas that must be kept free of encroachment so that the 1% annual chance flood can be carried without substantial increases in flood heights.

OTHER FLOOD AREAS

- ZONE X** Areas of 0.2% annual chance flood; areas of 1% annual chance flood with average depths of less than 1 foot or with drainage areas less than 1 square mile; and areas protected by levees from 1% annual chance flood.

OTHER AREAS

- ZONE X** Areas determined to be outside the 0.2% annual chance floodplain.
- ZONE D** Areas in which flood hazards are undetermined, but possible.

COASTAL BARRIER RESOURCES SYSTEM (CBRS) AREAS

OTHERWISE PROTECTED AREAS (OPAs)

CBRS areas and OPAs are normally located within or adjacent to Special Flood Hazard Areas.

- 1% annual chance floodplain boundary
- 0.2% annual chance floodplain boundary
- Floodway boundary
- Zone D boundary
- CBRS and OPA boundary
- Boundary dividing Special Flood Hazard Area Zones and boundary dividing Special Flood Hazard Areas of different Base Flood Elevations, flood depths or flood velocities.
- Base Flood Elevation line and value; elevation in feet* (EL 967)
- Base Flood Elevation value where uniform within zone; elevation in feet*

* Referenced to the North American Vertical Datum of 1988

- △ Cross section line
- ③ Limited detail cross section line
- ⊙ Transsect line
- 87°07'45", 32°22'30" Geographic coordinates referenced to the North American Datum of 1983 (NAD 83), Western Hemisphere
- 1000-meter Universal Transverse Mercator grid values, zone 18N
- 600000 FT 5000-foot grid ticks: New York State Plane coordinate system, East zone (FIPSZONE 3101), Transverse Mercator projection
- DX5510 x Bench mark (see explanation in Notes to Users section of this FIRM panel)
- M1.5 River Mile

MAP REPOSITORY
Refer to listing of Map Repositories on Map Index

EFFECTIVE DATE OF COUNTYWIDE FLOOD INSURANCE RATE MAP
September 28, 2007

EFFECTIVE DATE(S) OF REVISION(S) TO THIS PANEL

For community map revision history prior to countywide mapping, refer to the Community Map History table located in the Flood Insurance Study report for this jurisdiction.

To determine if flood insurance is available in this community, contact your Insurance agent or call the National Flood Insurance Program at 1-800-638-6620.

MAP SCALE 1" = 300'

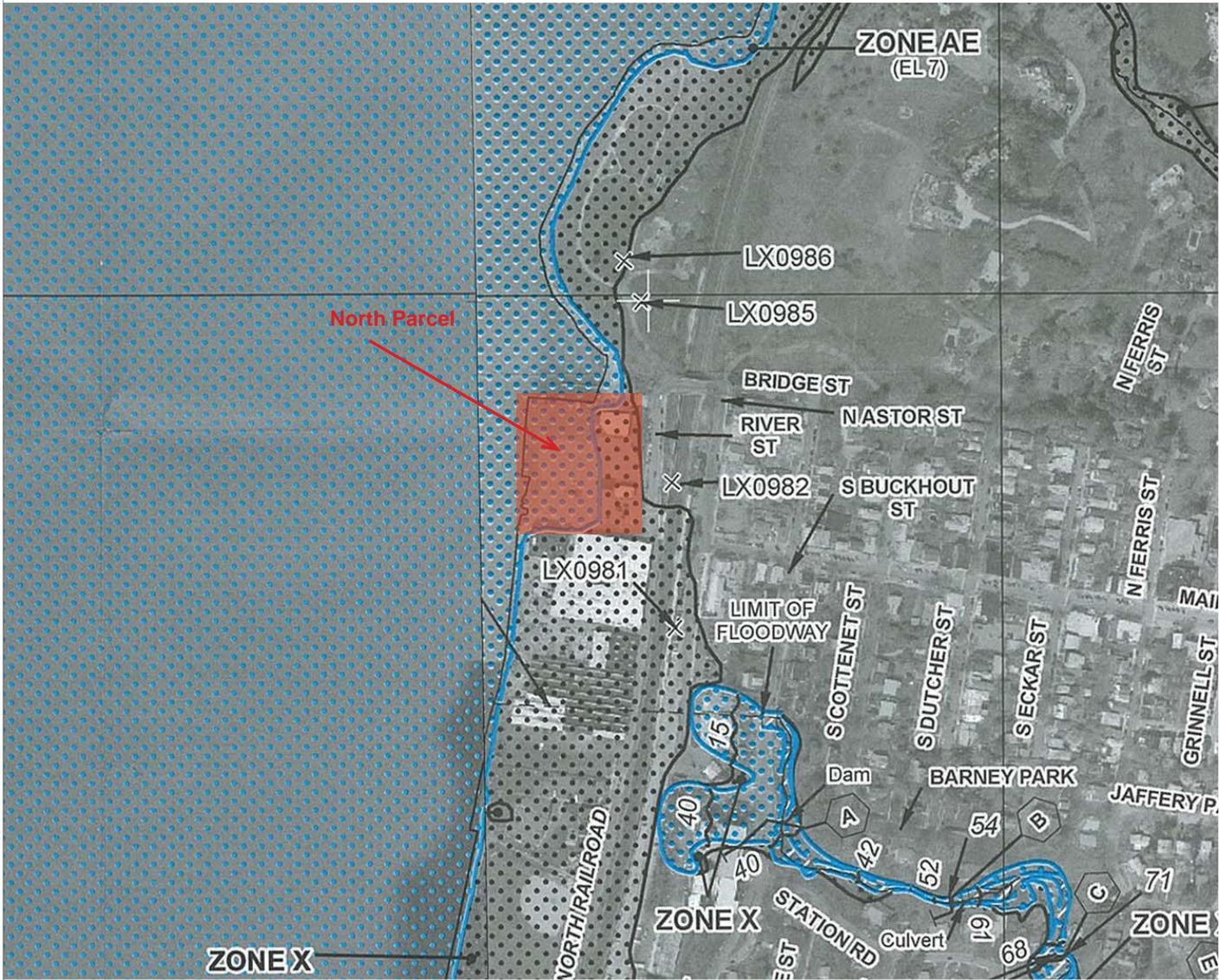


FIGURE 8: FEMA MAP

3.0 Bridge Street Properties Proposal

3.1 Visual Impact Analysis

The following Figures 10-15 provide a visual analysis of the Bridge Street Properties proposal. This includes:

Site Sections

- North-south site section through Bridge Street
- East-west site section with Bridge Street Context
- East-west section down Main Street

Photomontages

- North Astor Street – existing and proposed
- Cottenet Street – existing and proposed
- Matthiessen Park – existing and proposed

3.2 North Parcel Concept Plan Parking Summary

The parking calculations for the proposal are based on the proposed Mixed Use zone requirements and are summarized below. The new townhouse units will have 2-car parking garages, the duplexes will have 1-car garages with an additional space designated in the courtyard area. The retail space will be assigned 30 parking spaces in the new deck. The total number of spaces in the proposed deck is 226. We understand that this number was reduced to accommodate a retail building at the corner of Main and Bridge streets. The table below provides a summary:

Residential Uses

Use	Parking Req'd (under MU zone)	Proposed
13 townhouses	2 /D.U. = 26	26 garage 12 driveway
6 duplexes	2/D.U. = 12	6 garage 6 courtyard
		16 visitors in ctyrd.
	Total	66

Parking Deck (226 spaces)

Use	Parking Req'd (under MU zone)	Proposed
Retail (4,500 sq. ft.)	1/150 s.f. = 30	30
South Parcel Parking Req'ts	186	186
Additional Parking in Deck	$226 - (186 + 30) = 10$	10
	Total	226

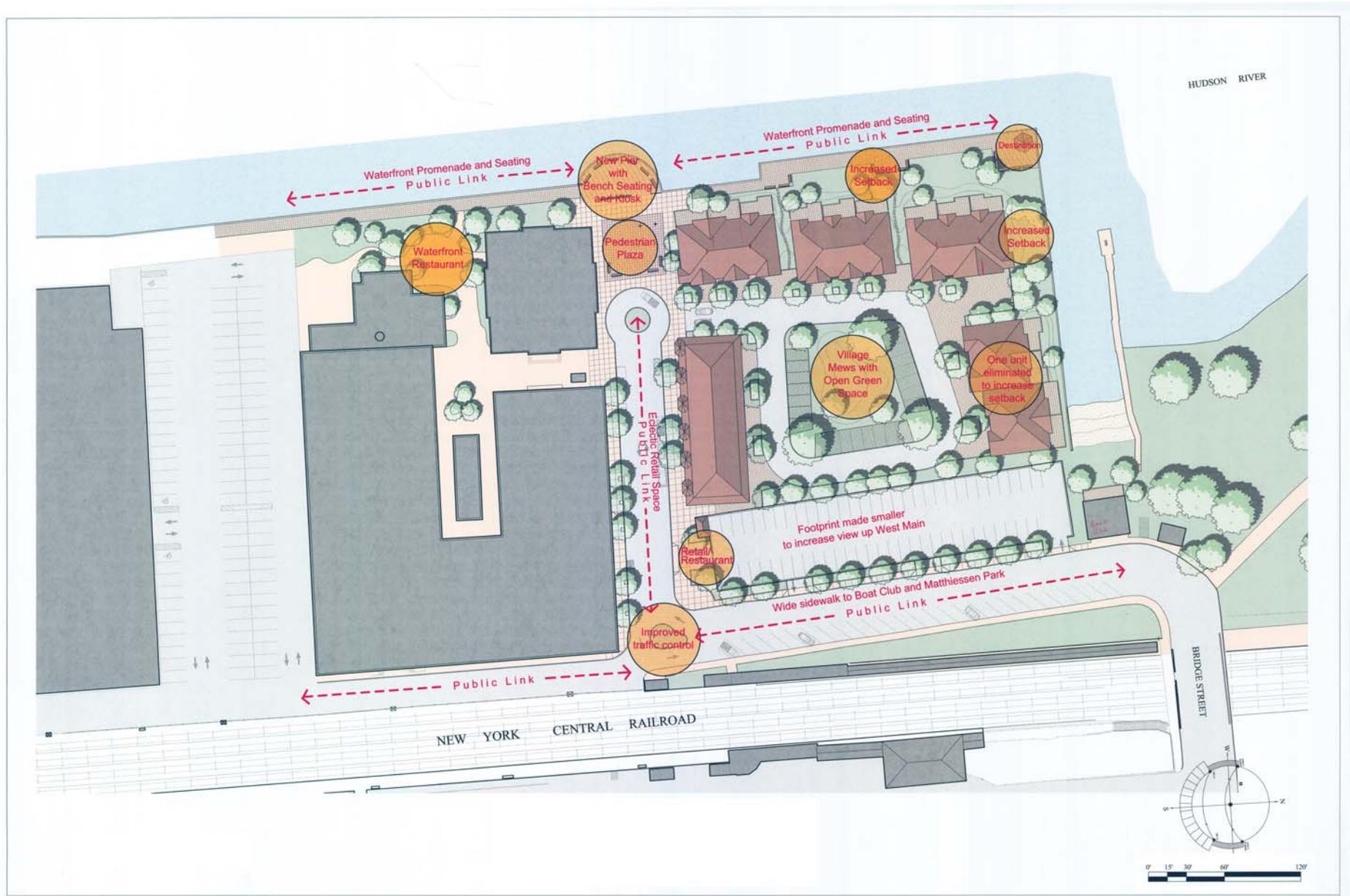


FIGURE 9: BRIDGE STREET PROPERTIES PROPOSAL

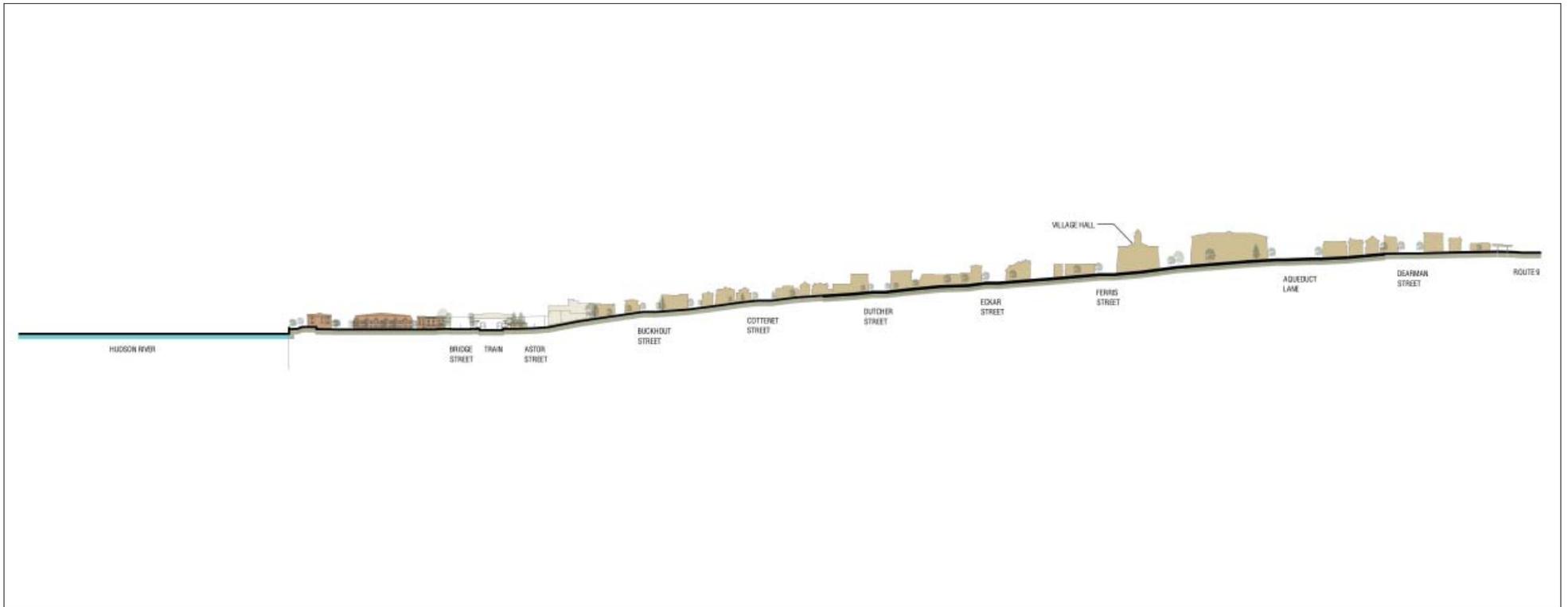


FIGURE 10: EAST-WEST SITE SECTION - MAIN STREET



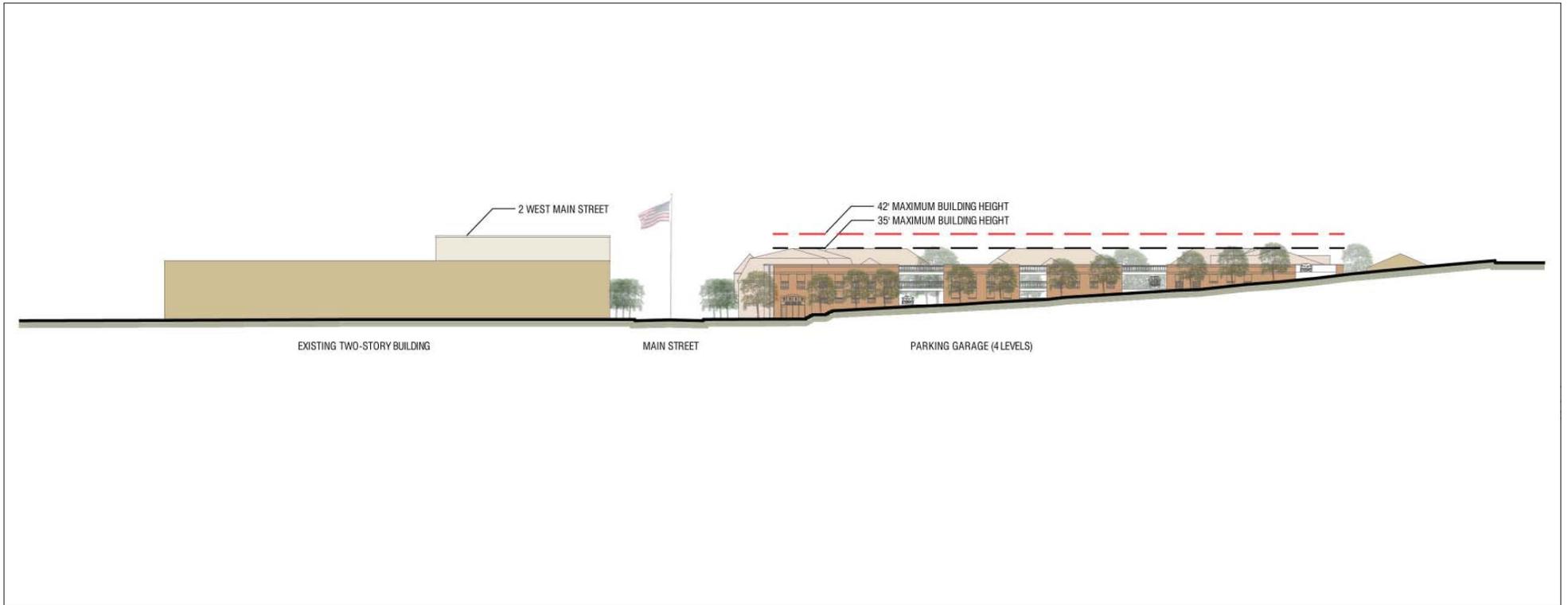


FIGURE 11: NORTH-SOUTH SITE SECTION - BRIDGE STREET

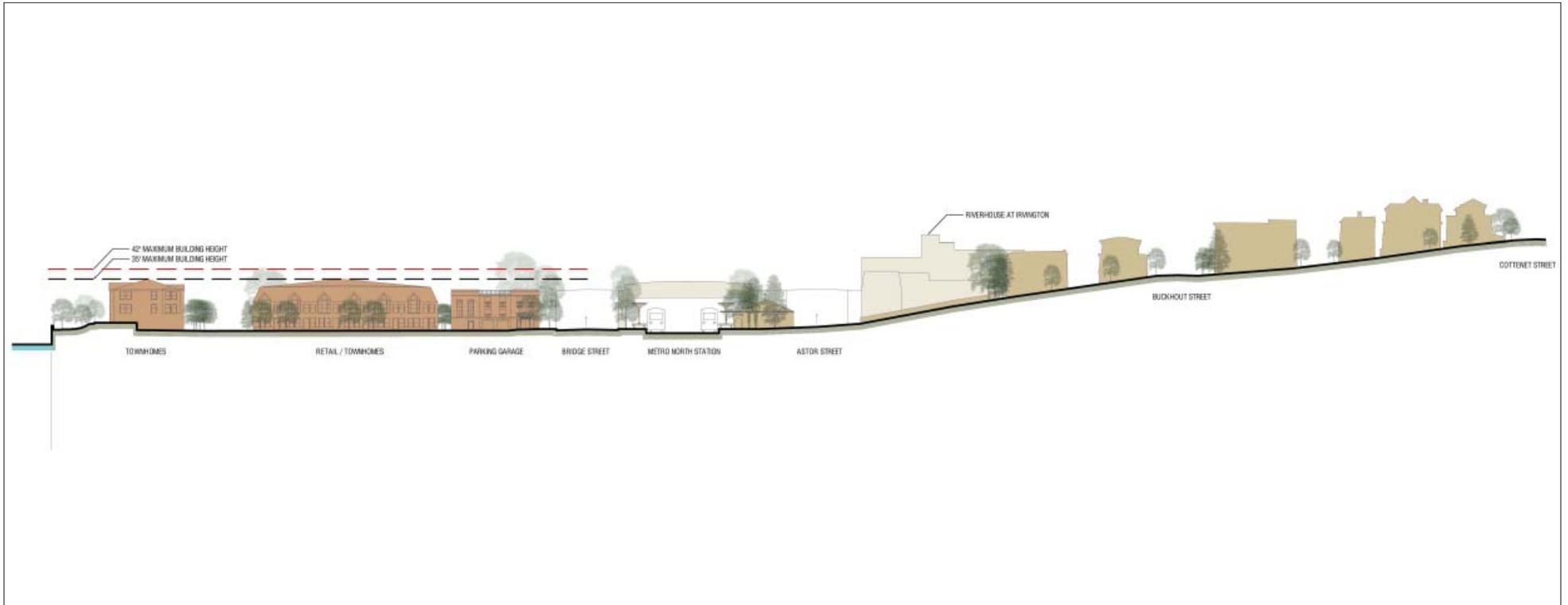


FIGURE 12: EAST-WEST SITE SECTION - BRIDGE STREET





FIGURE 13: GARAGE ELEVATION

Current View from Main Street



Main Street at Grinnel Street



Main Street at Eckar Street

Proposed View from Main Street



Main Street at Cottenet Street

FIGURE 14: MAIN STREET VISUAL IMPACT

Current View from Astor Street



Proposed View from Astor Street



FIGURE 15: ASTOR STREET VISUAL IMPACT



BEJ Planning

Current View from Mattiessen Park



Proposed View from Mattiessen Park



FIGURE 16: MATTIESSEN PARK VISUAL IMPACT



BEJ Planning

4.0 Review of Comparable Downtown and Waterfront Developments

The following provides a review of comparable new developments in Westchester County Villages. Overall, based on comparable developments, the concept plan for the North Parcel seems reasonable and even conservative in terms of overall height and scale.

1. Ferry Landings, Village of Tarrytown

Location: Hudson River waterfront, walking distance to train station
Uses: 250 apartments, commercial, retail, public open space
Height: 45 feet or 3 stories to midpoint of roof

2. Livingston Ridge, Village of Dobbs Ferry

Location: east side of railroad tracks, overlooking Hudson River
Units: 24 full-floor apartments
Height: 45 feet or 4 stories

3. Ichabod's Landing, Village of Sleepy Hollow

Location: waterfront parcel immediately south of old GM site
Units: 44 townhomes
Height: 65 feet

4. Half Moon Bay, Village of Croton-on-Hudson

Location: Hudson River waterfront
Units: 158 terrace and townhomes
Height: 37 feet and 3 stories, 25 feet and 2 stories within 75 feet of mean high water line

5. The Avalon Willow, Village of Bronxville

Location: downtown Bronxville, opposite the train station
Units: 110 apartments
Height: 42 feet and 3 stories

6. Christie Place, Village of Scarsdale

Location: downtown Scarsdale, opposite train station
Units: 42 apartments
Height: 46 feet and 4 stories

Village of Tarrytown



Ferry Landings Development



Site Plan



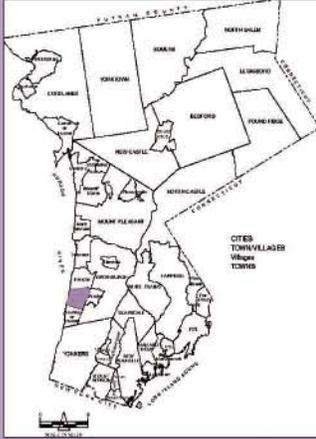
Elevation

Village of Tarrytown

- WGBD (Waterfront General Business District) and Waterfront District (WD)
- **Maximum Height: 45 feet to midpoint of roof, 3 stories**
- Height restriction may be waived with special permit and provision of specific amenities.

FIGURE 17: FERRY LANDINGS, VILLAGE OF TARRYTOWN

Village of Dobbs Ferry

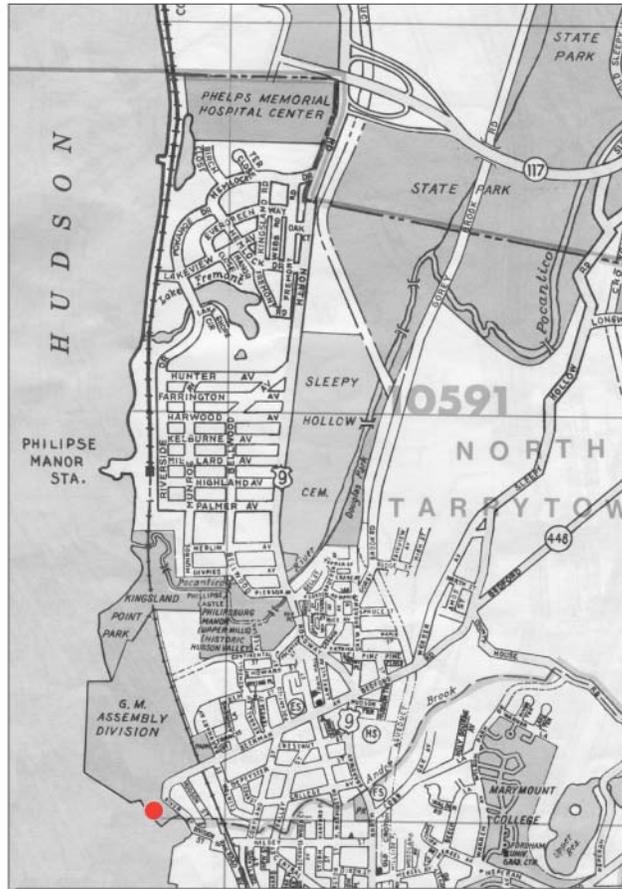


Village of Dobbs Ferry

- Draft LWRP includes proposed waterfront district as follows:
Maximum Height: 45 feet or 4 stories east of tracks

FIGURE 18: VILLAGE OF DOBBS FERRY

Village of Sleepy Hollow

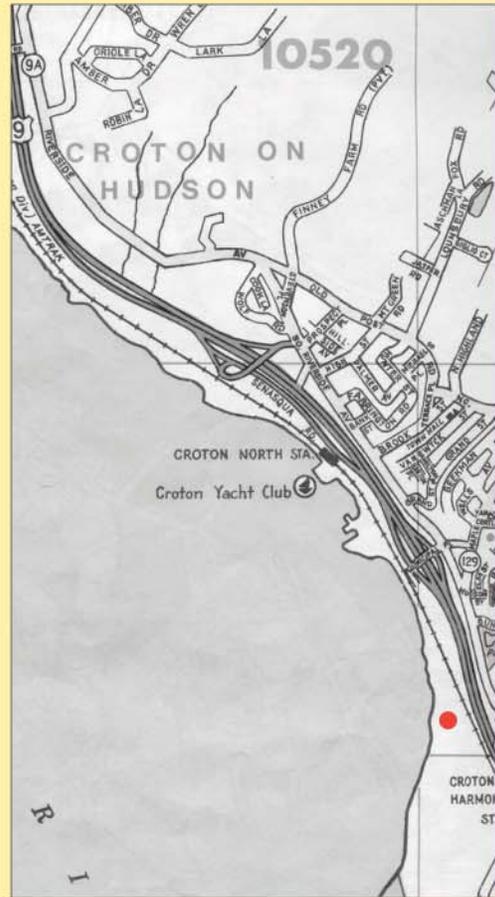


Village of Sleepy Hollow

- Riverfront Development Zoning District (RF)
- 65 ft

FIGURE 19: ICHABOD'S LANDING, VILLAGE OF SLEEPY HOLLOW

Village of Croton-on-Hudson



Half Moon Bay. 3 Stories.

Village of Croton on Hudson

- Waterfront Development (WD)
- **Maximum Height:** 37 feet and 3 stories
25 feet and 2 stories within 75 feet of mean high water line

FIGURE 20: HALF-MOON BAY, VILLAGE OF CROTON-ON-HUDSON

Village of Bronxville



The Avalon Development



Village of Bronxville

- Residence A (R-A) and Business B (B-B) District.
- **Maximum Height: R-A 35 ft and 2 1/2 stories
B-B 42 ft and 3 stories**

FIGURE 21: AVALON, VILLAGE OF BRONXVILLE

Village of Scarsdale



Christie Place Development

Village of Scarsdale

- Business District A and B (B-A & B-B) and Residence A-3 (RA-3)
- **Maximum Height: B-A & B-B 46 ft and 4 stories**

FIGURE 22: CHRISTIE PLACE, VILLAGE OF SCARSDALE

5.0 Recommendations

The following section describes recommendations for the redevelopment of the North Parcel. Section 6.0 of this report focuses new zoning to replace the Industrial District.

Public walkway: As currently proposed, the waterfront promenade extends the full length of the property terminating at the northwest corner. We understand that access to Matthiessen Park is restricted to the public; however we suggest that the promenade might be extended on the north edge of the property to connect to Bridge Street via a walkway in between the boat club and the parking garage. This may require the parking garage to be reconfigured to accommodate a walkway and would result in the elimination of approximately 8 parking spaces. Please see Figure 22. We also recommend that based on our experiences with comparable waterfront developments in similar communities, that the width of the public esplanade be a minimum of 15 feet. This may require a reconfiguration of the site plan.

New crosswalk: To provide a connection for the walkway suggested above and described on the attached plan, we recommend a crosswalk should be added at Bridge Street (see attached sketch). This will require eliminating some of the diagonal parking spaces.

New Sidewalk Connecting Bridge Street to Scenic Hudson Park: Another recommendation that is also contained in the Comprehensive Plan is to provide for safe pedestrian passage on Bridge Street south to Scenic Hudson Park. This might include a narrow sidewalk or paving leading from the train station underpass and crossing south towards Scenic Hudson Park.

Parking Deck: We recommend that the parking spaces within the deck should be at least 8 ½ feet wide and the deck itself can be up to 65 feet wide. 65 feet allows for two rows of parking (at 18 feet deep each) separated by a 22 foot wide drive aisle with an additional 5 feet to allow for wall thickness and some flexibility in the design of the exterior. As proposed the garage height appears to be 28 feet. This height results in unfeasibly low floor to ceiling heights. A more realistic height is 32 feet.

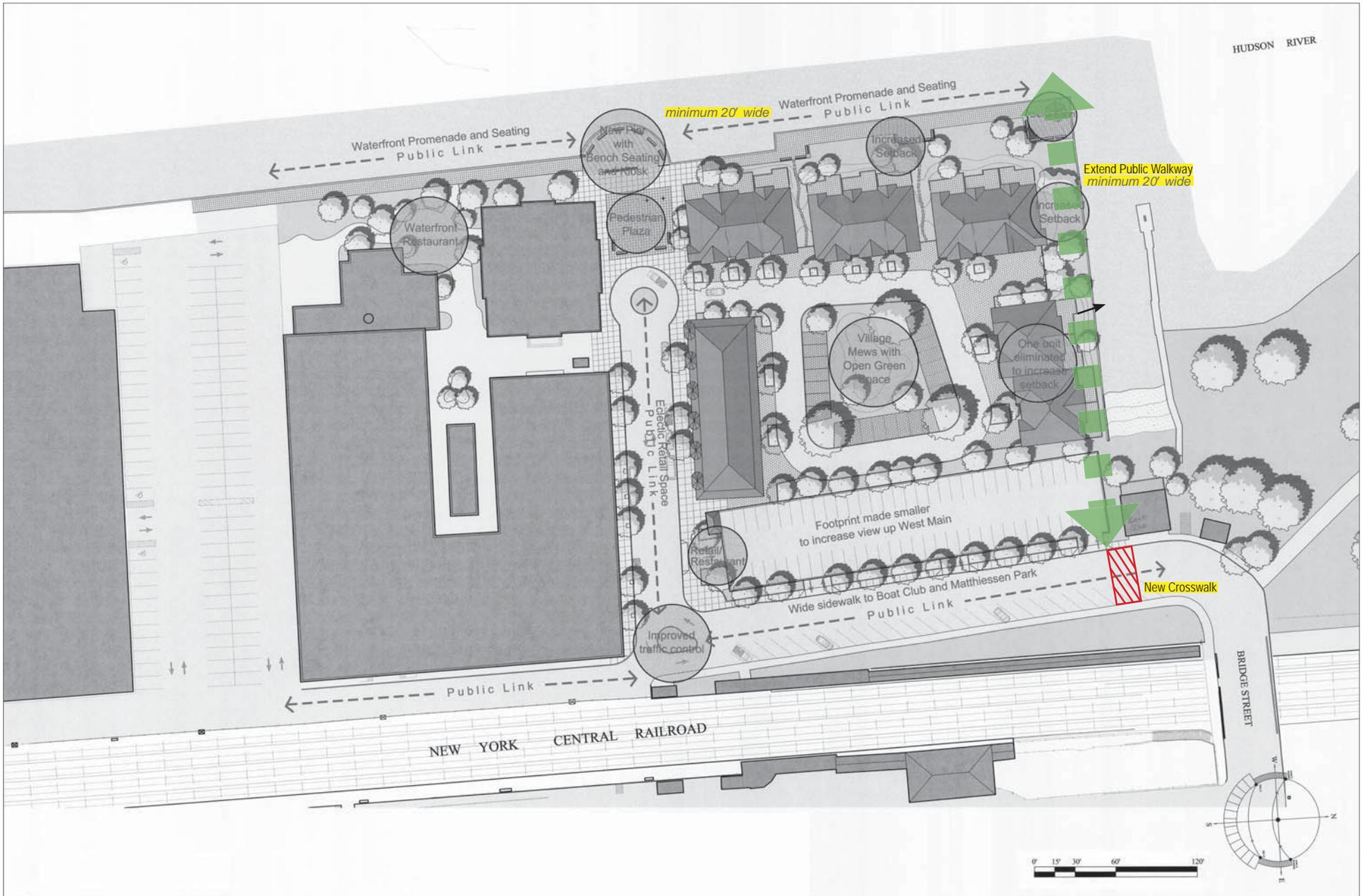


FIGURE 23: BFJ RECOMMENDATIONS

6.0 DRAFT Waterfront Mixed Use District (W)

Implement recommendations of 2003 Comprehensive Plan: Consistent with the Village's Comprehensive Plan we recommend that the existing Industrial zone be eliminated and replaced with a new Waterfront Mixed Use district (W). Below we provide a discussion of the proposed use regulations, bulk and area controls, setbacks, height requirements and waterfront esplanade.

1. **Uses Controls:** Existing Article X Industrial District (I) would be replaced with new Article X Waterfront Mixed Use District (W). Permitted uses would include retail, offices, art galleries and fitness clubs, municipal and public facilities. This is based on existing use controls within the Business B zone mapped on Main Street and would allow many of the existing uses within the Industrial zone to continue to operate and expand. We also propose residential uses to be a permitted use and limited to a density of 1 unit per 2,500 square feet of dry land area. This density is consistent with the existing residential density permitted in the Business District and would allow up to 40 units at the North Parcel. We also propose a list of special permit uses which would include parking structures with a list of conditions relating to bulk and views of the Hudson and Palisades. A category called product research and development, testing and assemblage is added based on uses currently within the Bridge Street properties.
2. **Floor area, height and coverage:** Floor area ratio is proposed at 1.4 and would be based on the dry lot area. This is the same FAR as is currently permitted in the Industrial District. Applied to the North Parcel, which is 2.4 acres, this would allow a maximum floor area of approximately 146,000 square feet which we believe is more than enough to accommodate the proposed concept plan. Bridge Street 1, to the immediate south and fronting West Main Street is built out at approximately 1.4 FAR and Bridge Street 2 to the south towards Scenic Hudson Park is currently built at approximately 0.35 FAR (based on 1 existing floor).

Height is proposed at 35 feet measured to the highest point of the roof, which is consistent with the existing code definition of how height is measured. In our experience height is more usually measured to the midpoint between the eaves and the peak of the roof if the roof is pitched, so we believe this is a conservative approach. Mechanical bulkheads or elevator overruns may exceed 35 feet by up to 7 feet provided they occupy no more than 5% of total roof area or lot coverage.

Lot coverage is proposed at 50% of the dry land area of the lot with an 80% limitation for impervious surfaces including buildings, parking and driveways. This is compared to 70% limitation in the existing Industrial zone. This will make the existing Bridge Street properties non-conforming as they are covered primarily with buildings and pavement.

3. **Yard and Setback Requirements:** No front yard is required. We recommend instead that buildings are set back at least 10 feet from Main Street and 5 feet from Bridge Street. The concept plans currently indicate a minimum 12 feet from

Main Street and 5 feet from Bridge Street. We recommend a minimum separation between buildings of 10 feet. The concept plan indicates 5 feet between the parking structure and mixed use building fronting on Main Street. We believe this is too narrow and should be adjusted. Rear yards would be 10 feet. Setbacks from the Hudson River are 25 feet with a 10 foot wide allowance for a private terrace. Parking structures must be set back 75 feet from the Hudson River.

4. **Waterfront Esplanade:** We propose a waterfront esplanade of at least 15 feet in width measured from the bulkhead line. 10 feet is permitted for pre-existing buildings.

Figure 24: Comparison of Existing Business and Industrial District Controls with Proposed Waterfront Mixed Use District (W)

Zoning Controls	Business District (B) Article IX	Industrial District (I) Article X	Waterfront Mixed Use (W) New Article X
Permitted Uses	Commercial, office, retail and restaurant uses Residential is permitted at 1 unit/2,500 sf of lot area, must be located 2 nd floor or higher	Industrial uses Residential by special permit	Water dependent uses Retail stores, banks not more than 5% of all bldgs. on lot, business, offices Residential at 1 unit /2,500 s.f. of dry land area of lot.
Floor Area Ratio	n/a (only applies to residential districts)	1.4 (excludes storage in basements, cellars)	1.4, based on dry land area of lot 0.6 FAR residential
Height	35 ft or three stories No higher than neighboring bldgs. on Main Street	35 ft or three stories	35 feet or three stories incl. parking structures 32 feet for parking structures
Coverage	Principal and accessory bldgs. 70% Incl. parking and driveways shall not exceed 80%	Principal and accessory bldgs. 70% Residential bldgs. 40%.	50% principal and accessory bldgs, 80% for bldgs. plus parking and driveways
Yard requirements	At least 4 ft wide, if req'd by Planning Board At least 10 ft wide abutting res. district.	None required, but, if provided any yard shall be at least 4 ft wide At least 10 ft wide abutting res. district.	Setback from Hudson River: 25 feet Private terraces may encroach 10 feet Min. 15 foot wide esplanade Min. 5 feet from Bridge Street Min. 10 feet from West Main Street Parking structures must be set back 75 feet from the Hudson River
Parking	1 space/1 bdrm, 1.5 spaces/2 bdrm., 2 spaces/3+bdrm. 15% credit w/in 600' of RR station tunnel 1 space/150 sq. ft. retail 1 space/200 sq. ft. office space Credit for 24' of permitted curb parking	1 space/DU 1 space/500 sq. ft. on first floor 1 space/375 sq. ft. on second and higher	2 spaces/DU 1 space/300 sq. ft. of retail/service 1 space/200 sq. ft. of medical, dental 1 space/400 sq. ft. of product design, testing Up to 25% reduction in total spaces with shared parking analysis 15% credit w/in 600' of RR station tunnel

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DRAFT ARTICLE X Waterfront Mixed Use District (W)

Replaces existing Article X Industrial District I

§ 224-39. Purpose and Intent.

The purpose of the waterfront mixed use district (W) is as follows:

1. To allow for the continued use of former industrial buildings for business, professional and commercial uses.
2. To encourage water-dependent and water related uses.
3. To promote public waterfront access including a waterfront esplanade.
4. To allow for new mixed use residential and commercial development limiting the bulk and height to preserve Village character.
5. To preserve views of the Hudson River and Palisades.

§ 224-40. Use regulations.

- A. No building or premises shall be used and no building or part of a building shall be erected which is arranged, intended or designed to be used, in whole or in part, for any purpose except the following:
 - (1) Water dependent uses. Including a fishing piers and small boat docks not to exceed 5,000 square feet.
 - (2) Retail stores not to exceed 5,000 square feet.
 - (3) Office uses.
 - (4) Personal service stores.
 - (5) Art galleries, fitness clubs.
 - (6) Municipal and public facilities.
 - (7) Dwelling units not to exceed 2,500 square feet of dry land area.
- B. Special permit uses; requirements.
 - (1) Boat marinas.
 - (2) Bed and breakfast, inns, hotels and clubs.
 - (3) Parking structure. Parking structures may be approved as a special permit use subject to the following conditions:
 - i. Setbacks. Parking structures must be set back a minimum of 75 feet from the mean high water mark of the Hudson River.
 - ii. Historic Context. The overall design of parking structures shall be considered in the context of existing historic buildings within the Waterfront Mixed Use District (W).
 - iii. Viewsheds. Consistent with Article XXIV View Preservation Overlay Districts (VP), the siting, dimensions and configuration of parking structures shall be designed to minimize visual impacts.
 - iv. Height and Width.

- a) Parking structures shall not exceed 32 feet in height with a 7 foot allowance for mechanical bulkheads. Mechanical bulkheads shall occupy no more than 5% of total roof area or if there is no roof, 5% of building coverage.
 - b) Parking structures shall not exceed 65 feet in width.
- (4) Banks, not to exceed more than 5% of all built floor area or 1,000 square feet.
 - (5) Theaters and restaurants, excluding fast food, amusement parks or galleries, whether open or enclosed or circuses.
 - (6) Outlets and pickup stations for laundries and cleaning establishments, excluding washing of wearing apparel or cleaning of wearing apparel or household effects other than where noncombustible solvent is used and where combustible solvent is used only for the incidental removal of spots.
 - (7) Public utility installations.
 - (8) Product design, research and development, testing and assemblage.

C. The following uses shall be prohibited:

- (1) Any use which is noxious or offensive by reason of emission of odor, dust, noise, smoke, gas, fumes or radiation or which presents a hazard to public health or safety.
- (2) The use of premises for the operation of any fast-food establishment or any restaurant or food service business providing curbside, drive-through or window-counter service or having more than 1/2 parking space for each seat therein.

§ 224-41. Floor area, height and coverage.

- A. Floor area. No building shall be erected or enlarged in which the aggregate floor area, exclusive of cellar and basement areas used only for storage or for the operation and maintenance of the building, shall exceed 1.4 times the area of the lot on which such building stands including all levels of any parking structures. The floor area calculation shall be based on the dry land area of the lot and shall include all levels of any parking structure.
- B. Height. No part of any building shall be erected to a height greater than three stories at any point along the periphery of such building, nor shall such height exceed 35 feet. Mechanical bulkheads may exceed the maximum height of 35 feet by up to 7 feet provided that they occupy no more than 5% of total roof area or if there is no roof, 5% of building coverage.
- C. Lot coverage. The sum of all areas covered by all principal and accessory buildings shall not exceed 50% of the dry land area of the lot. The sum of all areas covered by all principal and accessory buildings, parking and driveways shall not exceed 80% of the dry-land area of the lot, calculated in both cases from the mean-high water line of the bulkhead.

§ 224-41. FEMA and US Army Corps Requirements

New developments within the Waterfront Mixed Use (W) district must comply with the provisions of Chapter 124 Flood Damage Prevention of this code and any other applicable regulations including Federal Emergency Management Agency (FEMA) flood maps (effective date 9/28/07) and Army Corps of Engineers jurisdiction.

§ 224-43. Yard and setback requirements.

Yards.

- A. Front yard. None required. However, all buildings, including parking structures, shall be setback a minimum of 10 feet from Main Street and 5 feet from Bridge Street.
- B. Side yard. No side yards are required. However, the minimum separation between buildings is 10 feet.
- C. Rear yard. The rear yard shall be a minimum of 10 feet.

Setbacks from Hudson River.

- A. All buildings must be set back a minimum of 25 feet from the Hudson River. No structures shall be permitted within this setback except for private terraces up to a maximum of 10 feet. See Section 224-42 below for waterfront esplanade requirements.

§ 224-44. Waterfront Esplanade

Consistent with the provisions of Section 224-163 Greenway Compact Plan any new development within the Waterfront Mixed Use (W) zone must provide a publicly accessible waterfront esplanade consistent with the requirements of this section.

- A. The waterfront esplanade must be at least 15 feet wide measured from the bulkhead. 10 feet is permitted in those areas of the district where buildings were in existence prior to the date of the adoption of this zoning text. The esplanade must be open and unobstructed.

§ 224-45. Signs, awnings and canopies.

Signs, excluding billboards, awnings and canopies shall be permitted in the Waterfront Mixed Use District (W) consistent with the provisions of Section 224-36 (11).

§ 224-46. Off-street parking.

A. Parking.

- (1) Product design, research and development, testing and assemblage uses shall provide at least one parking space for each 400 square feet.
- (2) On-site parking spaces shall be provided as follows, in addition, if applicable, to the requirements of § 224-55:
 - a) One space per 200 square feet of retail store, personal service store.
 - b) One space per 300 square feet for office uses, except that medical and dental offices must provide one space per 200 square feet.
 - c) One space per one-bedroom dwelling unit.
 - d) One and one-half spaces per two-bedroom dwelling unit.
 - e) Two spaces per three-or-more-bedroom dwelling unit.
- (3) The following shall also apply:
 - (1) The parking requirement may be reduced by one space for each 24 feet of curb space, where street parking is permitted, along the frontage of the lot.

- (2) The parking requirement for all dwelling units within 600 yards of the Irvington Railroad Station tunnel entrance shall be reduced by 15%.
- (3) For lots of 5,000 square feet or smaller, the Planning Board may, in its discretion, in lieu of parking spaces as required by this chapter, allow the lot developer to pay a fee, which shall be a minimum of \$10,000 per space, into a parking supply fund to be established and administered by the Board of Trustees.
- (4) No parking space or vehicle storage area shall be situated between the curblineline of West Main Street and the facade of any building fronting, facing or abutting West Main Street.
- (5) No more than 20% of the area of any lot having frontage on Main Street may be used for parking spaces or vehicle storage areas.
- (6) Shared Parking. The Planning Board may reduce the total number of parking spaces required by up to 25% based upon review of a shared parking analysis submitted by the Applicant.

B. Loading. Loading spaces shall be provided as follows:

- (1) All office space: none required for floor space up through 25,000 square feet; one space for floor space exceeding 25,000 square feet up through 50,000 square feet, plus one space for each 25,000 square feet, or any portion thereof, thereafter.
- (2) Retail: none required for floor space up through 8,000 square feet; one space for floor space exceeding 8,000 square feet, up through 20,000 square feet, plus one space for each 20,000 square feet, or any portion thereof, thereafter.

Article II Definitions

New text would be added to define shared parking as follows:

PARKING, SHARED – Joint utilization of a parking area for more than one use. Peak parking demand for each use should be at different times of day for greatest efficiency. Within specified zoning districts, the Planning Board may reduce the total number of parking spaces required by up to 25% based upon review of a shared parking analysis.

Appendix:

Waterfront Tax Map

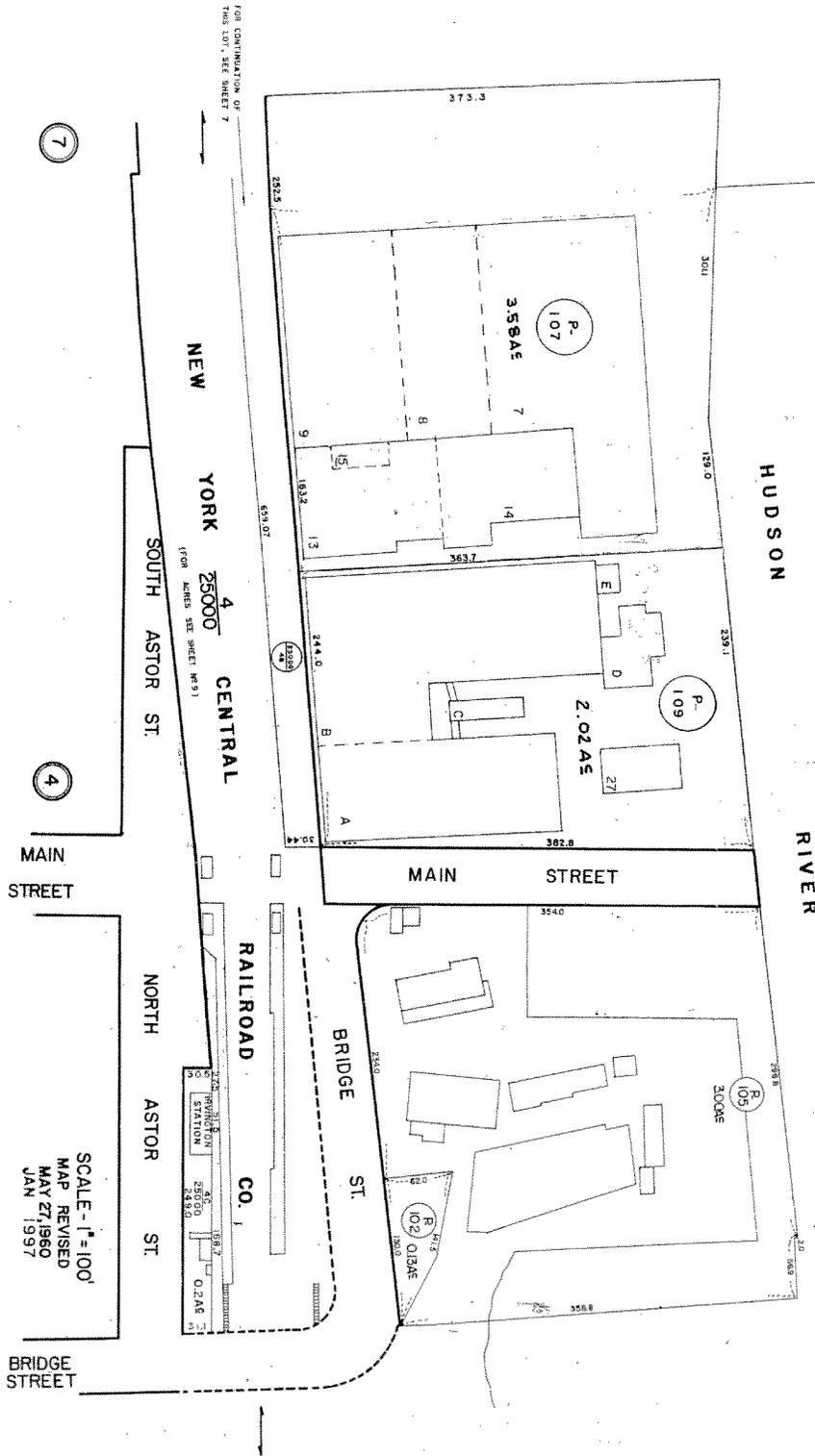
Notes from Special Waterfront Meeting, June 1, 2006

Parking Deck Analysis

SECTION 4

3

NEW 2001		OLD 2000	
SHT	BLK.	LOT	LOT(S)
3	---	P-107	pt P103, SHT-7-P89
3	---	P-109	pt P103



NOTES FROM THURSDAY, JUNE 1, 2006
SPECIAL MEETING OF THE
VILLAGE BOARD
HELD IN THE IRVINGTON PUBLIC LIBRARY

OPPORTUNITIES

Expansiveness
Tappan Zee
Natural

View From All Sides

180 degrees
Wind
Noise of water
Parking space
Place to run

LEAST LIKE

Lack of activity
No reason to go there
Feels Private
Desolate
Paved experience and pollution run off
Ugly
No access to water
No seating
Want boat access/transportation
No foliage/greenery
No sense of place

WHO

Dog walker
Roller Bladder
Shoppers
Diners
Parents of baseball players nearby
Water uses
Performance viewers
Public art watchers
Strollers
Walkers to Matthiessen
Swimmers
Commuters

USES

Swimming
Walking
Performing arts – concerts, movie, street theater
Sports rental
Farmers market
Ice cream shops
Coffee
Kiosks
Kayaking
Boat launching

ACCESS & LINKAGES

Waterfront walkway from Scenic to Matthiessen
Shuttle to alternate parking and East Main Street School parking lot at night
Boat/Kayak launch?
Water taxi to other towns
Continuation of Main Street sidewalks
Sidewalks all the way down

CHARACTER & IDENTITY

Warm & inviting
Hot district or exclusive
Colorful
Natural
Artistic
Activities day & night
Safe

AMENITIES OR FEATURES

Benches
Sitting areas
Foliage – low profile
Trolley/shuttle
Waterfront access walkway
Pedestrian lighting
Low profile parking garage
Water fountain
Public bathrooms
Restraints
Passive enjoyment
Public art in tunnel or sculpture

VIEWS TO PRESERVE

Top of Main

Middle of Main

View of Train Station

See Manhattan from North Mattheissen

Street grid

1. **WHAT TO WE LIKE?**
 - Views of Manhattan – Palisades
 - Accessibility to water
 - Social ability conducive to hanging out
 - Openness
 - Potential for other uses - scooters – roller blading
 - Boating – Boat Club – access to water

2. **DISLIKES**
 - Asphalt
 - Odors from restaurant dumpsters
 - Not access existing parking lot
 - Limited parking to Matthiessen & riverfront parking

3. **WHO SHOULD BE ABLE TO USE THIS SPACE?**
 - Open to the public
 - Anybody
 - Water enthusiasts
 - Families

4. **USES AND ACTIVITIES**
 - Restaurants
 - Boat Club – criteria for public access
 - Boat Launch
 - Better boat ramp – compares to riverfront para
 - Bookstore
 - Professional offices (2nd Floor)
 - Sports equipment
 - Space/kiosk for live music
 - Need second access way
 - Limit residential
 - Public promenade for access to the river – Bide riding – read a book

5. **ACCESS & LINKAGES**
 - Pedestrian Street – allow access for deliveries
 - Parking for commercial use e.g. restaurants
 - Open gate get park and riverfront properties
 - Zen garden
 - Better signage to encourage traffic (pedestrian and vehicles)
 - Sidewalks
 - Gazebo – on the water – North end of promenade

6. **COMFORT/IMAGE**

- Eclectic
- Continuation/expansion of Main Street character
- Consistent style/character of Main Street integrate the waterfront and East Main Street
- Green space/plantings
- Benches along waterfront
- Cafes facing the waterfront!
- Keep the waterfront public
- Do not privatize the waterfront

7.

8. **HOW CAN WATERFRONT PRESERVE COMMUNITY**

• **GOALS**

- a. Views – NYC Skyline
 - Palisades South
 - Piermont Marsh
 - River expanse
 - River view from Main Street
 - Preserve current resident views
- b.
 - Provide access to late evening hours
 - Keep promenade open accessible like Main Street
 - Special character – regional designation – not a strong consensus
- c. Rear street
 - Pedestrian orientation
- d. Historic character
 - Not a separate enclave
 - Integrate with East Main Street
 - Make existing West Main Street businesses more public
 - Similar uses and character for East and West Main Street
 - Preserve existing Brier Building – relocate?
- e. Parking Needs
 - How did new building get build to 42 feet height?!

HEIGHT

- View down the river
- Do not want to “over shadowing” feeling
- Do not want wall of building looking south from Matthiessen
- Want to see sunsets

PARKING

- Not commuter parking
- Need more Matthiessen parking
- Maximize parking at south end of Bridge street more parking availability for Village residents at north end

LIKE BEST

1. Views from Main Street
From Matthiessen Park
Openness – feeling, river's edge
Access – fishing, parking, biking – Irvington Boat Club
Commercial uses – restaurants

LIKE LEAST

2. Vacant ness, asphalt, impervious amenities
Underdeveloped – no sidewalk, trees, benches
Nothing near water to enjoy
View blocked by new building on south
No direct access to water
Pollution (salt & oil) from lot
Heavy traffic on bridge

SOCIABILITY

3. Community use – shopping/recreation
Children, families, teens

USES & Activities\

4. Recreation - Looking at river – water & Land (passive)
Gazebo (concerts) coffee house
Bazaar – Crafts fair
Farmers market
Dining
Boat rental (canoe/kayak)
Markets
Boating – excursion etc. water taxi
High-end retail (art/antiques)

ACCESS LINKAGE

5. Footbridge over tracks South of Main Street
Open station (remove fence)
Promenade connect both parks
Improve alley between track & BSP
Access by boat from river signage!!!

CHARACTER/IDENDITY

6. Remain as Main Street/Burnham/etc. as historic district (signage).
Personable/human scale
Hudson Riverfront community
Charming, vibrant, inviting
Attract the eye

AMENITIES

- 7. Benches
 - Low impact lighting
 - Comfort station
 - Pier (fishing, sitting, docking)
 - Shade/trees/awnings
 - Quiet area
 - Fountain
 - Tables/chairs
 - Landscaping – native plants
 - Bike/skate path (bike racks)

VIEWS

- 8. a. Palisades, sunset, NYC skyline, river/palisades from Main – up Main Street from riverfront area from areas north of Main Street

DESTINATION

- b. Events to draw at all times – weekend programs
 - Open to attract general public – signage/ access/convenient parking/ footbridge

WEST MAIN

- c. Tree lined with set backs
 - Landscaped sidewalks
 - Make a pedestrian walkway/plaza

HISTORIC CHARACTER

- d. Maintain consistency with historic district (facades, roofline, building materials, lighting)

PARKING

- e. Bike racks, low visibility parking – further south multilevel parking away from waterfront
 - Trolley, more pedestrian facilities

INTEGRATE

- f. Connect the parks with promenade
 - Provide pier for docking & fishing
 - Provide access for waterfront recreation
 - Enhance views (Belvedere)

OTHER GOALS

- g. Integrate development into community
 - Inclusive, not exclusive
 - Pay for itself tax-wise
 - Safety

1. **LIKE**

- Can drive to fishing spot and park
- Boat Club has motorized boat access
- Un obstructed views
- Can sit at Solera and look at river
- Relatively quite
- Architectural appearance of (new) building
- Bridge Street existing appearance and uses

2. **DISLIKE**

- Lack of parking for Matthiessen
- Unattractive open parking lot

BFJ Planning

MEMORANDUM

To: Larry Schoffer and Waterfront Zoning Committee
From: Frank Fish, Paul Buckhurst, Tom Yardley
Subject: Parking Decks
Date: March 28, 2008

To follow-up our meeting of March 10 we have explored the possibility of a second parking deck location between the two existing buildings owned by Bridge Street Properties. Paul Buckhurst and George Jacquemart have suggested a one deck structure between the existing buildings and Paul has illustrated this on the attached drawings.

South Lot Deck

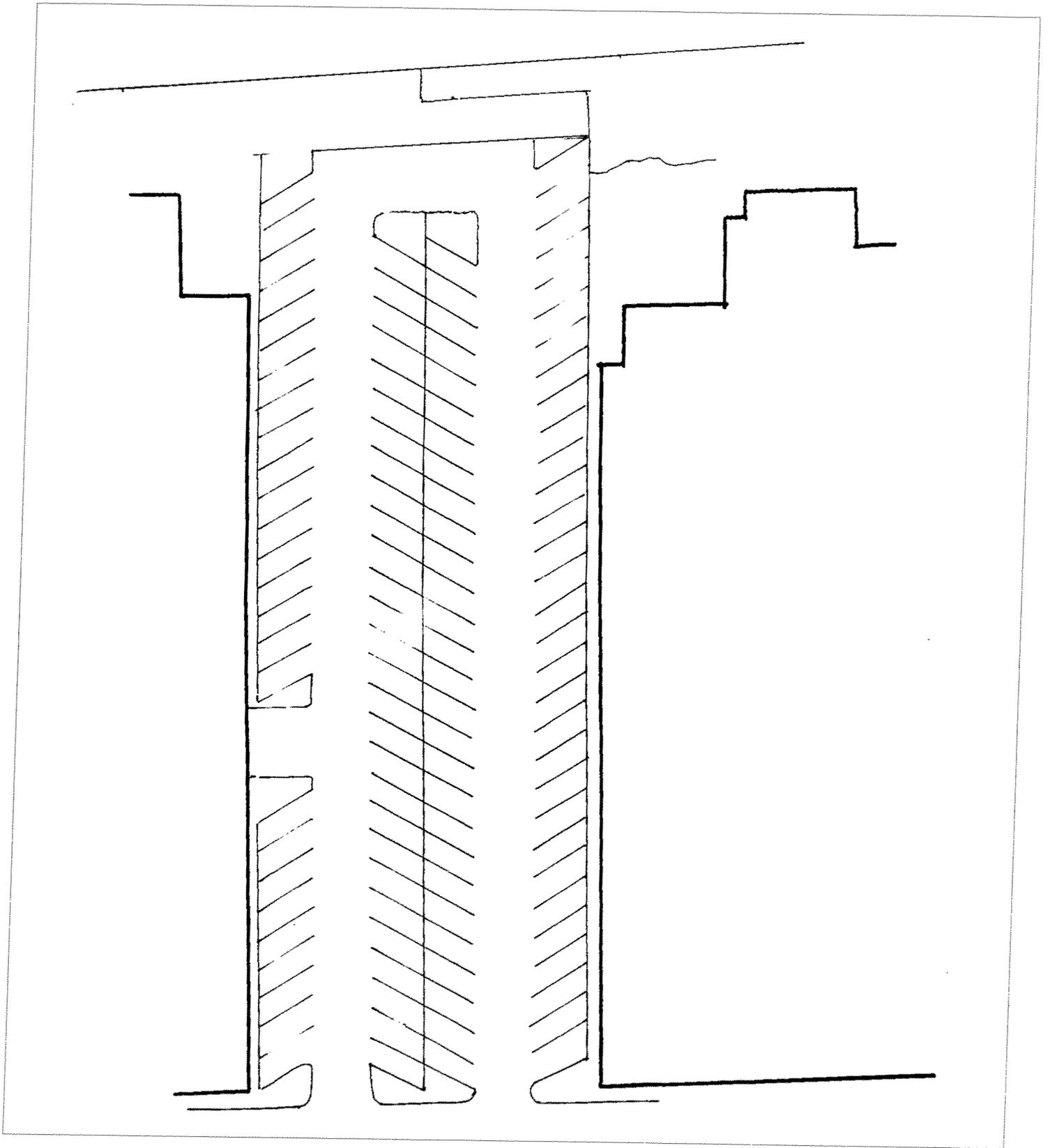
This allows for two aisles giving access to diagonal parking bays as shown in figure 1. Adding a parking deck at this location poses a number of problems:

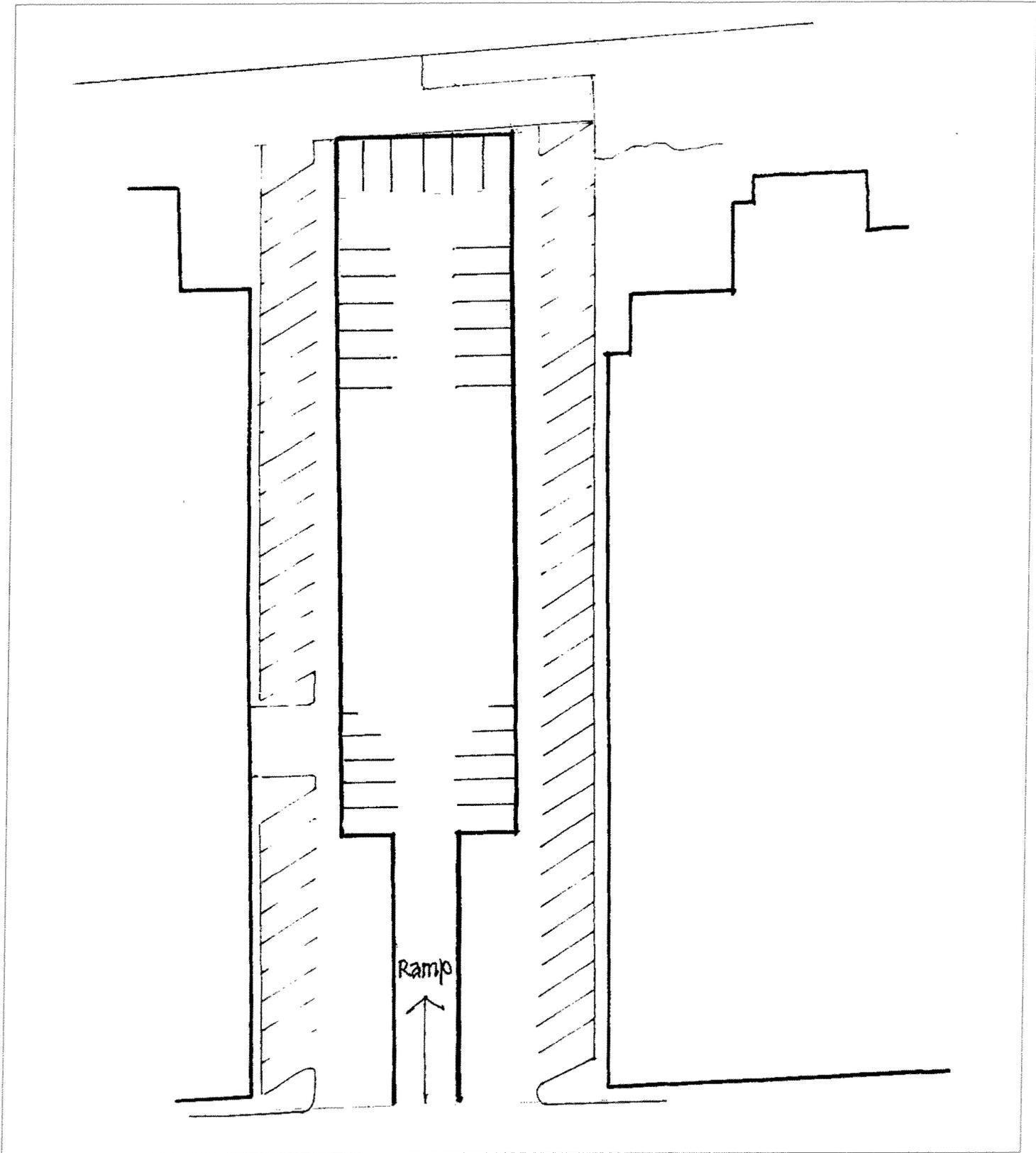
- The deck width is constrained by the need to maintain daylight to adjacent buildings
 - Approximately 16 surface spaces are lost due to the need of the entry/exit ramp to the deck
 - Additional existing 8 spaces are likely to be lost due to the need for column supports for the deck.
- Figure 2 illustrates a possible one-level deck solution that provides approximately 56 spaces. The net gain is a maximum of only 40 spaces given the loss of existing spaces due to the ramp and the support columns. This total compares to the 60 spaces provided on each level of the proposed Bridge Street garage.

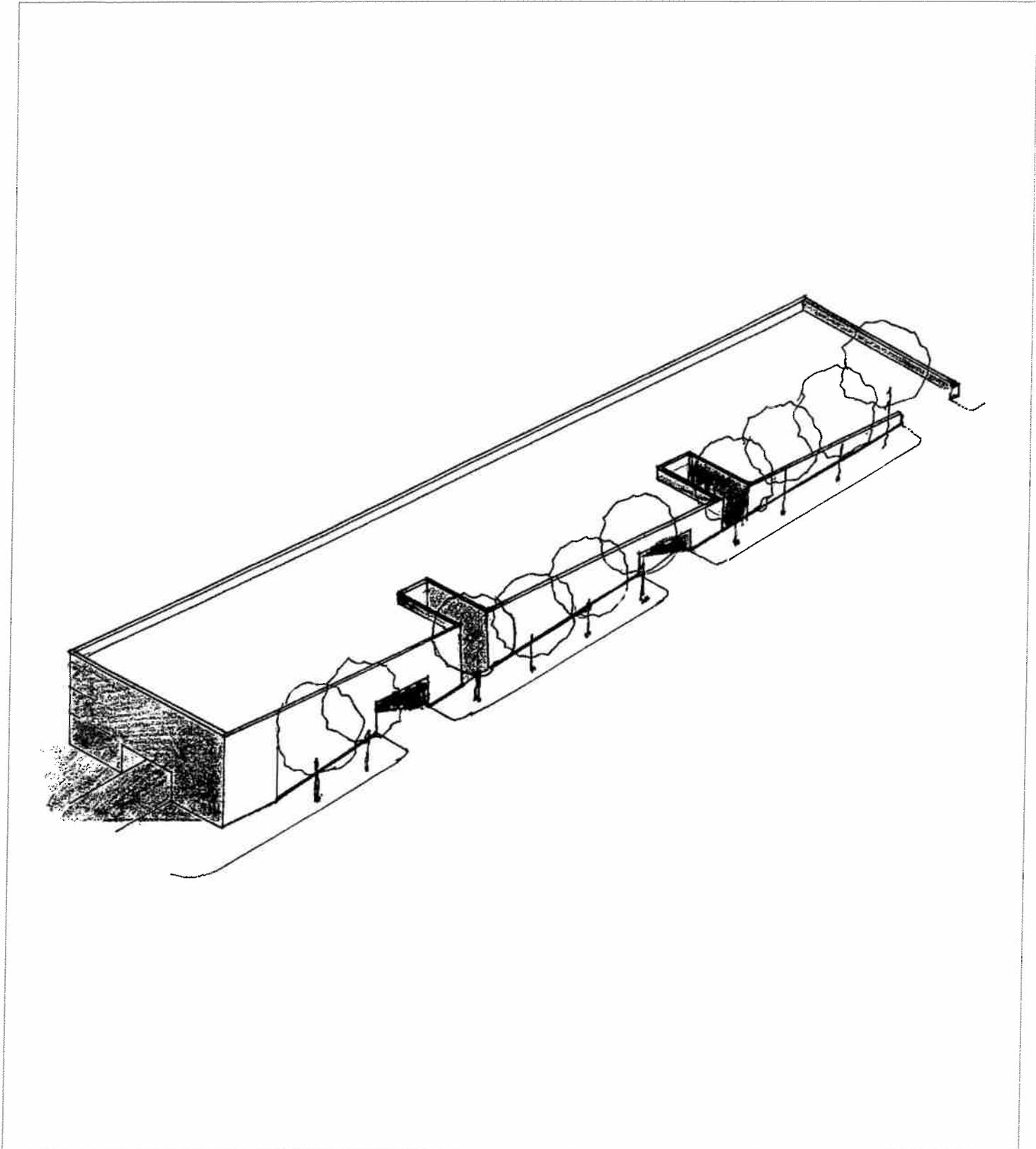
The cost of the new deck would be at least \$25,000 per space or approximately \$1,400,000.00 (25000x56 spaces). However the net cost per new space (\$1.4m/40) is \$35,000/space. This does not assume any costs for piles which may be needed in this area. We will need to ask the applicant for additional subsurface information to see whether piles would be necessary. Given the decks proximity to the bulkhead we assume they may be needed at least at the western end.

Elevation Treatment of Proposed Garage

A second alternative is to attempt further design improvements to the proposed garage. The current proposal by Peter Gisolfi shows that the garage elevation is slightly below the peak of the roofs of the proposed housing. While we believe that the floor to ceiling heights in the garage may need to be slightly increased the top garage level is still within a compatible height with the proposed housing. The proposed elevational treatment facing Bridge Street provides for a series of different-scaled openings and bay treatments that go a long way to reducing the visual scale of the approximately 280 feet long building. Figure 3 illustrates an additional feature that could be considered to further articulate the building's East façade. This incorporates two 20 feet deep recesses (ie two parking bays) that would break up the bulk of the building and also act as light wells to the decks below grade level. This feature would result in the loss of 8 spaces out of a proposed total of approximately 240 spaces.







Acknowledgements

BFJ Planning

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Paul Buckhurst, Principal, Design
Tom Yardley, Project Manager

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Mamaroneck, NY 10543