

**VILLAGE OF IRVINGTON**  
**AD HOC TRANSPORTATION COMMITTEE**

**REPORT TO TRUSTEES**

**APRIL 1, 2005**

# VILLAGE OF IRVINGTON

## AD HOC TRANSPORTATION COMMITTEE

### REPORT TO TRUSTEES

April 1, 2005

Dear Mayor and Members of the Board of Trustees:

The Ad Hoc Transportation Committee has met 12 times over the past 15 months and its members have earnestly reviewed and discussed various aspects of the issues with which they were charged. The following is a brief summary of the committee's recommendations:

- Create a "SCHOOL ZONE", with a 15-mph speed limit on Main Street between Aqueduct Lane and Ferris Street.
- Consider installing raised crosswalks across Main Street on the east side of its intersection with Ferris Street (by Village Hall) and the west side of its intersection with Croton Place (by the Main Street School).
- Provide painted crosswalks on all sides of the intersections along Main Street. High-visibility, zebra-type crosswalks should be provided across Main Street and Astor Street.
- Install a STOP sign on the Main Street approach to Astor Street, making this intersection an all-way stop intersection.
- Replace and supplement existing signing on Cyrus Field Road with new, high-reflectivity signs and post strips.
- Ensure that all crossings of the Aqueduct are adequately signed for both motorists and pedestrians.
- Request that the State Department of Transportation fix the traffic signals at the intersections of Broadway with Sunnyside Lane, Main Street and Langdon Avenue so that the green arrow indications only come up if there is a vehicle waiting to turn in the designated left-turn lane facing the signal.
- Request the State Department of Transportation to permit the Village to reduce the speed limit on the north and south ends of Broadway to 30 mph.
- Implement the proposed Main Street Residential Parking Program on a trial basis.
- Permit overnight parking on the south side of Main Street, east of the Aqueduct Lot.
- Restripe the Aqueduct Lot with Perpendicular spaces to provide nine (9) more spaces.
- Provide one (1) additional parking spaces on North Aqueduct Lane.
- Provide angled and perpendicular parking on the east side of North Astor Street to eliminate the current undesirable practice of double parking at that location.
- Provide one (1) additional parking space at the north end of Municipal Parking Lot # 5.
- Investigate the possibility of providing Village employee parking over the old sewer plant on South Buckhout Street.
- Contact All-Rite to see if a better arrangement could be worked out that would increase off-hour parking supply in the vicinity Matthiessen Park.
- Explore all reasonable opportunities to further increase parking in the Main Street area.

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**A. Introduction**

For your benefit, provided below is a brief summary of the committee's understanding of the various charges set forth by you, followed by a report on the committee's work and details of the committee's recommendations to date:

1. The Village should have a current official map that correctly identifies all public, private and paper roads and streets within the Village as a planning tool that allows the Village to see what transportation infrastructure it has and how changes in the Village might affect this infrastructure. The committee should undertake whatever work it can to facilitate the preparation of the new official map.
2. The committee should review and scrutinize the various elements of the suggested transportation infrastructure codification presented in the Comprehensive Plan to see if they are sensible. If so, the committee should send up to the Board of Trustees detailed recommendations on the issue by street and by code-type for legislation.
3. The committee should review and scrutinize the various restrictive measures identified in the Comprehensive Plan that would be applied to the various classes and categories of roads established in Item 2 above to see if they are sensible. If so, the committee should send up to the Board of Trustees detailed recommendations on each of the identified measures or suggest others for legislation.
4. The committee should investigate areas where traffic operating conditions are perceived to be undesirable and identify effective traffic calming measures, including policies for implementation that would permit the Village to easily identify whether the identified measures were or were not applicable at various locations.
5. The committee should review the needs and problems associated with parking in the Main Street area, including the possibility of a resident parking permit scheme, and make recommendations to improve parking and safety. As part of this process, the committee should review previous studies and observations on file on such matters, including the Masterson Parking Study Plan and the recent 2000 Main Street Study.
6. The committee should identify transportation and recreational centers in the Village that are difficult to access on foot as well as by bike and make recommendations on how access could be improved.

Following the Committee's initial meeting, it was agreed that the issues identified in Item 4 and Item 5, previously described, represent a current potential risk to the general public welfare and should be tackled by the committee first.

## **B. Traffic Calming**

One of the four main goals of the 2003 Village Comprehensive Plan was “protecting the health, safety and quality of life of Village residents” (page 7), while one of the specific policy objectives of the Transportation section of the Comprehensive Plan’s Executive Summary was to “enhance pedestrian and bicycle networks” (page 9). Implementing traffic calming measures was one of the programs identified in the Comprehensive Plan to achieve the stated goal and objective.

### **B.1 Recommendations**

After discussing the need for traffic calming, where it might be warranted, what measures could be implemented and what criteria would apply for a determination of need, the committee recommends the following initial measures:

- 1 It was noted that Main Street, in the vicinity of the school, meets all the criteria for the establishment of a “SCHOOL ZONE”, which would enable the Village to enact a 15-mph speed limit at that location (Aqueduct Lane to Ferris Street) during school hours. It was generally agreed by the committee that, due to the presence of a police officer in the morning and afternoon, the benefits of such a zone would be greatest in the evenings, when after-school activities let out.

The committee recommends that the Board of Trustees establish a 15-mph SCHOOL ZONE on Main Street between Aqueduct Lane and Ferris Street using signs only (see attached). This measure would remind motorists of the presence of school children along this section of Main Street and would also highlight the frequent presence of school-aged pedestrians at the Town Hall Theater on weekends.

- 2 The Board of Trustees should consider installing raised crosswalks across Main Street on the east side of its intersection with Ferris Street (by Village Hall) and the west side of its intersection with Croton Place (By the Main Street School), retaining an engineering firm to assist if the Board deems it advisable. The purpose of these crosswalks is to provide a pedestrian connection between the north and south sides of Main Street at these heavily-trafficked locations, to remind motorists of the presence of pedestrians throughout the Main Street corridor and to require motorists on Main Street to travel at more pedestrian-friendly speeds. The expected reduction in the average travel speed along Main Street between Aqueduct Lane and Eckar Street will also benefit motorists turning out of the intervening side streets as well as motorists parking and unparking.

The crosswalks should be level with the sidewalks on either side of the street, be approximately eight feet wide, have gently sloping sides, be plowable in the winter and should allow for proper drainage. If the crosswalks can be designed to satisfy these

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- criteria, the committee recommends their construction at the aforementioned locations. If successful, similar crosswalks could be installed at other high-volume pedestrian locations.
3. The Board of Trustees should direct the Department of Public Works to provide painted crosswalks on all sides of the intersections along Main Street. North-south crosswalks (across Main Street) should be the high-visibility, zebra-type crosswalks presently found at some locations, while east-west crosswalks (across the side streets) should be two parallel painted white lines (except at the intersection of Main Street with Astor Place, where all three crosswalks should be high-visibility). The purpose of these crosswalks is to remind motorists of the presence of pedestrians and to advise pedestrians of the appropriate places to cross.
  4. The Board of Trustees should direct the Department of Public Works to install a STOP sign on the Main Street approach to Astor Street, making this intersection an all-way stop intersection. The STOP sign should be placed approximately 5 feet west (downhill) of the existing fire hydrant and the bottom of the sign should be 80 inches above the pavement for optimum visibility. Further, a stop bar and the letters S-T-O-P should be painted on the pavement at the end of Main Street. This measure will make it easier for motorists to cross Main Street at that location and will also make it safer for motorists to turn left onto Main Street from North Astor Street.
  5. Chief Grieco indicated that, in his experience, there are an increased number of accidents on Cyrus Field Road during wet weather. Mr. Canning and Chief Grieco separately conducted an inventory of current signing on Cyrus Field Road and both reported that, in their opinions, additional signing would likely improve traffic safety. Based on an inspection, it is recommended that all existing signing from the Village Line to the intersection of Fieldpoint Road with Harriman Road should be replaced new high-intensity reflective signs and that high-intensity reflective strips should be applied to the existing sign posts. Furthermore, additional chevron signs should be placed on the guiderail on either side of Fieldpoint Road at its intersection with Harriman Road (W1-13 signs on the Broadway side and W1-14 signs on the reservoir side, see attached ). It is further recommended that the following signs be placed on the right side of the roadway, facing traffic headed toward the Saw Mill Parkway:
    - A. A serpentine-road-ahead sign (W1-9, see attached) over a slippery-when-wet sign (W4-8, see attached), with a supplementary panel below (W9-2, see attached) that reads, "NEXT 1 MILE" and a high-intensity reflective strip on the post on Harriman Road as it passes in front of the reservoir, approximately 35 feet before the first residential driveway after Fieldpoint Road.

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- B. A curve-right sign (W1-4, see attached), with a 20-mph supplementary panel (W9-1, see attached) and a high-intensity reflective strip on the post on Cyrus Field Road approximately 200 feet in advance of the first significant curve to the right in the road after Manor Pond Lane.

The following signing improvements are recommended on the right side of the Cyrus Field Road facing traffic headed toward the Village:

- A. A serpentine-road-ahead sign (W1-10, see attached) over a slippery-when-wet sign (W4-8, see attached), with a supplementary panel below (W9-2, see attached) that reads, "NEXT 1 MILE" and a high-intensity reflective strip on the post, approximately 350 feet north of the Dobbs Ferry Line.
  - B. Replace the 15-mph supplementary panel on the first curve-left sign encountered after passing Manor Pond Lane when headed toward the Village with a 20-mph supplementary panel (W9-1, see attached).
  - C. Replace the second curve-left sign encountered after passing Manor Pond Lane when headed toward the Village with a new curve-left sign (W1-3, see attached) over a slippery-when-wet sign (W4-8, see attached), with a 15-mph supplemental panel below (W9-1, see attached) and a high-intensity reflective strip on the post approximately 150 feet south (uphill) of the existing sign to be replaced.
6. The Board of Trustees should direct the Department of Public Works to check all street crossings of the Aqueduct for consistent signing warning of Aqueduct activity and the Board of Trustees should request the State Parks Department to install warning signs on the Aqueduct approaches to the crossing streets, advising Aqueduct users of crossing motor traffic.
  7. The Board of Trustees should request that the State Department of Transportation fix the traffic signals at the intersections of Broadway with Sunnyside Lane, Main Street and Langdon Avenue so that the green arrow indications only come up if there is a vehicle waiting to turn in the designated left-turn lane.
  8. Hand in hand with the implementation of these traffic calming measures the Board of Trustees should undertake an effort to inform motorists of the reasons for the measures so that they will receive more widespread support. Further, the Board should make efforts to increase the public's awareness of the dangers of speeding, advising the public that speeding on Village streets is unacceptable behavior and that the Police Department will continue to respond to specific concerns.

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Once the traffic calming measures identified above have been implemented for a sufficient period for their benefits to take effect, the ad hoc transportation committee should reevaluate the need for further traffic calming measures.

B.2 Rejected Considerations

One of the traffic calming concepts that was considered and rejected was providing mini traffic circles on Main Street at Cottenet Street and Astor Street to slow Main Street vehicles down, to facilitate access to Main Street from the side streets and to provide a location for westbound Main Street traffic to turn around (U-turn). Concern was expressed that, due to the high volume of traffic on Main Street and the considerably smaller volumes of traffic on the side streets, the suggested mini-circle would not work. It was further noted that the suggested mini-circle at Astor Street would eliminate the ability for vehicles to pull over and discharge train passengers or wait to pick up train passengers at that location, that there was no convenient alternate location to accommodate this activity and that there was no apparent problem at this location.

The committee was unaware on any significant volume of complaints or accidents relating to cut-through traffic on Sycamore Lane and Parkside Way. It was, therefore, concluded that any recommended traffic calming measures on these streets might be more objectionable than the current condition.

The committee discussed the possibility of installing three-way STOP control on Harriman Road at Cedar Ridge/Cedarlawn Road/Hillside Terrace to better allow motorists turning left out of the minor street approach to see eastbound traffic on Harriman Road and as a traffic calming measure on Harriman Road itself. The committee concurred with Chief Grieco's recommendation against this measure after he indicated that there have not been many accidents at this intersection, that most motorists probably turn right out onto Harriman Road, and that it was an inappropriate use of STOP signs at this location at this time.

The possibility of restricting westbound movement on Bridge Street at North Astor Street to left turns only was discussed as a possible means of reducing the volume of through traffic on North Buckhout Street. Since the Bridge Street bridge over the railroad is scheduled to be demolished and reconstructed, the committee determined that it would not be appropriate to implement any changes at this location at this time.

The Aqueduct was acknowledged by the committee as the most recognizable pedestrian and bicycle corridor/connection in the Village. It was noted that control of the crossings of the Aqueduct varied by location. Because pedestrians and bicyclists are more vulnerable than motorists, it was concluded that it would not be in their interest to provide a false sense of security by allowing Aqueduct users right-of-way at all street crossings (if a cyclist were to assume right-of-way and not slow in crossing at the same time that a motor vehicle failed to stop, the cyclist would have the most to lose). Similarly, it would inconvenience everyone to make all-way STOPs at all Aqueduct crossings.

**C. Reducing the Speed Limit on Broadway**

The committee has determined that there are a total of 18 streets and driveways on either end of Broadway in the 40-mph zone (primarily on the east side) that have limited sight distances. A review of the most recent three years of accident records for Broadway revealed that there were less than half as many accidents on the 40-mph sections of Broadway as on the 30-mph sections, but that you were twice as likely to end up in hospital if you were involved in an accident in the 40-mph section, as compared to the 30-mph section. From a review of this data, it was concluded that the 40-mph sections of Broadway are relatively safe, hence the lower number of accidents, but that the accidents that do occur are more severe, probably because of the higher rate of speed (the severity of an accident is a function of the square of the speed). The committee believes that if the speed limit is reduced to 30 mph, the risk of serious injury resulting from accidents on the effected sections will be reduced. Reducing the speed limit would also improve access to Broadway from the side streets and driveways along the effected sections. For these reasons, it is recommended that the Board petition the State to reduce the speed limit to 30 mph.

The Chief provided a "Smart Survey Info" printout, which showed that the average speed on northbound Broadway at Dows Lane (posted speed limit 30 mph, four-lane cross section) was 31 mph, while 85 percent of motorists were traveling at 37 mph or lower. Despite repeated attempts, the Chief was unable to obtain speed information from the 40-mph sections of Broadway at the north and south ends of the Village. Nonetheless, based on the experience of some committee members, it is expected that the average speed on these sections of Broadway is approximately 40 mph, with 85 percent of motorists expected to travel at speeds below 46 mph. In the event the speed limit were reduced to 30 mph, one could expect that travel speeds would be similar to but slightly higher than those measured on Broadway at Dows Lane, due to better grade and alignment. Thus, it is expected that reducing the posted speed limit to 30 mph would precipitate an approximately eight mile per hour reduction in speed. Affecting 0.75 miles at the south end of the Village, 0.35 miles at the north end of the Village and, possibly, 0.9 miles in the Village of Tarrytown, if implemented, the proposed reduction in speed is calculated to increase travel time in Irvington by 25 seconds and a possible further 20 seconds in Tarrytown.

It is noted that the committee is concerned that if the safety aspect of the 40-mph speed limit is over emphasized, the State may end up restriping the road to one lane in each direction, resulting in undesirable congestion. It is suggested that the Board of Trustees request a meeting between the Mayor, the Police Chief (also Tarrytown's Mayor and Police Chief if they so desire) and Mr. Richard Dillmann, P.E., Acting NYSDOT Region 8 Traffic Engineer. At that meeting, the Village should request that the speed limit on Broadway be reduced to 30 mph, making it consistent with the remainder of Broadway between New York City and the Pocantico River in Sleepy Hollow, emphasizing that it is the wish of the residents, that it will increase travel times by only 25 to 45 seconds and that it will make for a safer corridor, more in keeping with the character of the communities which it connects. Hopefully this will persuade the State to grant the request without changing the configuration of Broadway.

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**D. Parking**

**D.1 Main Street Residential Parking Permit Program**

The committee was actively involved in drafting the language for the initial legislation enacting the Main Street Residential Parking Program. The committee's deliberations reflected on the needs of the residents and merchants of the area and noted the complexity of the situation where each side street on Main Street presents a somewhat unique set of circumstances. The committee notes that it has no quantitative data to indicate the significance of the impacts of the proposed program but, on the whole, the committee feels that, absent any obvious reasons not to proceed, the legislation should be implemented. However, because of the wide variety of circumstances on the individual streets in the parking district, there could be a wide variety in the results of the program. The committee, therefore, recommends that the ordinance be revised to include a sunset provision which would require that the ordinance be evaluated and re-adopted within one year, otherwise the program would be discontinued. The committee further recommends that the text of the ordinance be revised to prohibit commercial vehicles with more than four (4) wheels from obtaining permits.

With regard to the specifics of the implementation of the program, the committee suggests that:

1. Where 6-hour time limits exist in the residential parking district, they should be retained;
2. Consistent with most of the other streets in the district, a 6-hour time limit should be imposed on those residential streets in the district which currently have no parking time limit;
3. The 2-hour time limit on Croton Place, Grinell Street and the east end of Home Place should be retained unless there is a compelling reason to change it.

The committee agreed that, for the program to work, it would need a minimum of administrative oversight and enforcement. To that end, it was suggested that the application for a permit consist of information such as the permit vehicle's licence plate (including whether it is a private or commercial plate), the number of wheels on the vehicle, the primary operator's drivers licence number, the primary operator's address (with proof of residence - Utility bill and copy of lease, if renting), an indication of the number of vehicles in the household, an indication of the number of driveway spaces available to the household and the number of other permits obtained by the household, along with the applicant's signature, declaring that the information provided is correct.

**D.2 Overnight Parking**

The committee recommends that overnight parking be permitted on those streets in the residential parking district where there is an expressed interest by the residents of the specific streets for such action. At this point in time, it is apparent that the south side of Main Street, east of the Aqueduct Lot is the only location where additional overnight parking should be allowed.

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The committee discussed the need for prohibiting the overnight parking of commercial vehicles on some or all Village streets and agreed that, while it could be a significantly irksome issue, at this point in time, other methods of dealing with this infrequent problem have been successful. Therefore, until such time as more authoritative measures are required, the committee suggests that there is not a need for such an ordinance.

### D.3 Aqueduct Lot Parking

Following an inspection and *de facto* test of providing perpendicular parking in the Aqueduct Municipal Parking Lot, the committee recommends that the parking lot be restriped to provide 8'-8" wide perpendicular spaces. With both handicapped parking spaces sharing a common 8-foot wide access aisle in the north west corner of the lot, the number of spaces in the lot can be increased from 46 to 55 parking spaces (28 on one side and 27 plus the handicapped access aisle on the other). It is further recommended that some "COMPACT CARS ONLY" signs be posted on the east side of the north end of the lot to account for the lot's reduced width at this location by encouraging motorists of the largest of vehicles to park elsewhere in the lot. It is also noted that providing perpendicular parking spaces will make it easier to turn around to get out of the lot when it is close to full.

### D.3 North Aqueduct Lane Parking

Following an inspection of parking on North Aqueduct Lane, the committee recommends that the last six feet of space at the south end of the current parking area between the two existing utility poles should be paved and that eleven (11), 8'-6" wide (perpendicular to the stripes) spaces be created, resulting in one additional space at this location.

### D.4 North Astor Street Parking

To eliminate the current undesirable practice of double-parking opposite the old train station building without reducing the number of parking spaces provided at that location, the committee recommends that the five parallel parking spaces on the east side of the street before it narrows to go up the hill to the bridge (in front of Fufi's and Mane Station) be restriped to provide angled and perpendicular parking spaces as shown on the attached sketch.

### D.5 Municipal Parking Lot # 5

The committee recommends that the Board of Trustees direct the Department of Public Works to construct one additional perpendicular parking space at the far north end of Municipal Parking Lot # 5.

### D.6 Old Sewer Plant

Based on a review of the area over the old sewer plant, which is located immediately west of Municipal Lot # 5, the committee recommends that the Board consider providing three-deep Village Employee Parking across the roof of the old plant. Most important to this decision is the certification that the roof of the sewer plant can accommodate the additional load associated with

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parking. Should this be feasible, it will allow an additional 18 vehicles to be parked at this location and will free up approximately 5 parking spaces on South Astor Street and a further five parking spaces on South Buckhout Street.

### D.7 South Buckhout Street Parking

The committee discussed the possibility of providing angled parking on South Buckhout Street, specifically opposite the Trent building. However, considering the potential safety hazard of motorists weaving back and forth around vehicles parked in different orientations while traveling north on South Buckhout Street, as well as little evidence of a shortage of parking at this location, the committee does not recommend that any parking changes be implemented at this location.

### D.8 Station Road Parking

The committee does not recommend that any parking changes be implemented in the vicinity of Station Road Park unless the retaining wall between the park and the road is moved back sufficiently to allow angled or perpendicular parking without vehicles sticking out into the roadway. Further, any parking provided on the east (uphill) side of the park gate should be parallel or angled parking so as not to create a potential conflict with vehicles turning off of Broadway onto Station Road.

### D.9 Bridge Street Parking

The requirement of payment of \$4 to park in the All-Rite parking spaces on Bridge Street and West Main, apparently even in the evening, overnight and on weekends was questioned. It is suggested that the Village contact All-Rite to see if a better arrangement could be worked out that would continue to allow commuter parking in these spots but make them more attractive to users of Matthiessen Park in the afternoon, evening and on weekends.

### D.10 Additional Parking on Center Main Street

The parking surveys conducted for the 2000 Main Street Traffic and Parking Study revealed that, while there were at least 20 percent more parking spaces than parked vehicles in the Main Street business district, motorists sometimes had to walk up to three blocks from their car to their destination and that some residents of South Dutcher as well as North and South and Eckar Streets could not find spaces to park overnight on their streets. While the need for additional parking in the Main Street area is primarily a matter of convenience and will be partly alleviated by the restriping of the Aqueduct Lot, the committee, nonetheless, concludes that it will become more pressing with the passage of time. Therefore, the committee recommends that the Trustees explore any reasonable opportunities to provide some additional parking in the Main Street corridor, preferably between Dutcher Street and Ferris Street (or within a half a block of this area) to provide more convenient parking for local businesses and to relieve the overnight parking crunch on Dutcher and Eckar.

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One such option would be an agreement with the Sunnyside Federal Savings and Loan (similar to the agreement between the Village of Tarrytown and the bank at the bottom of Neperan Road on Broadway) to allow the public to use some or all of the bank's spaces overnight and perhaps even some during the day, depending on the bank's needs. In the meantime, it is recommended that the frequency of parking complaints in the center Main Street area be monitored.

**E. Miscellaneous**

It is noted that, as a result of the committee's investigations, some minor adjustments were requested of the sanitation pick-up schedule on Main Street to further reduce the conflict between trash pick-up and commuters heading for the train.

Following the committee's completion of its efforts on Item 4 and Item 5 of its charge, the committee proposes, with the Board of Trustees consent, to proceed to Item 1 of its charge. That is, undertake whatever work it can to facilitate the preparation of a new official map that correctly identifies all public, private and paper roads and streets in the Village.

We look forward to discussing these preliminary recommendations with you at some time in the near future.

Sincerely,

The Irvington Ad Hoc Transportation Committee.

John Canning, Chair  
Don Marra, Village Administrator  
Louis Grieco, Chief of Police  
Peter Bernstein  
Mark Keefe  
Camille LeFevre  
Terry Masterson  
Pat Matero  
Ruth Neuwirth  
George Rowe  
Sasha Wilson

SCHOOL

SPEED  
LIMIT  
15

7:30 AM - 6 PM  
SCHOOL DAYS



W1-9



W1-10

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W1-3



W1-4



W4-8



W4-8

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W4-8



W9-1



W9-2

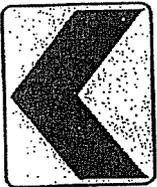
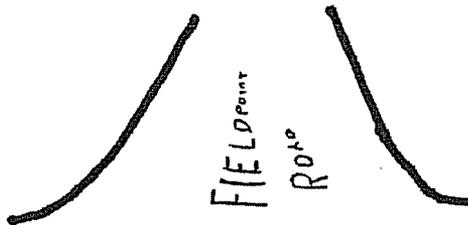


W9-2

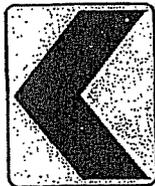
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W9-1



W1-13

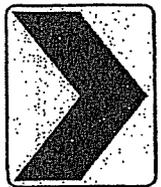


W1-13

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W1-14



W1-14

